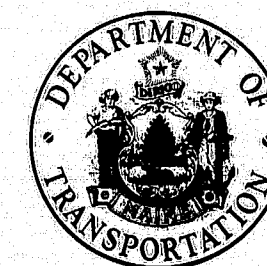
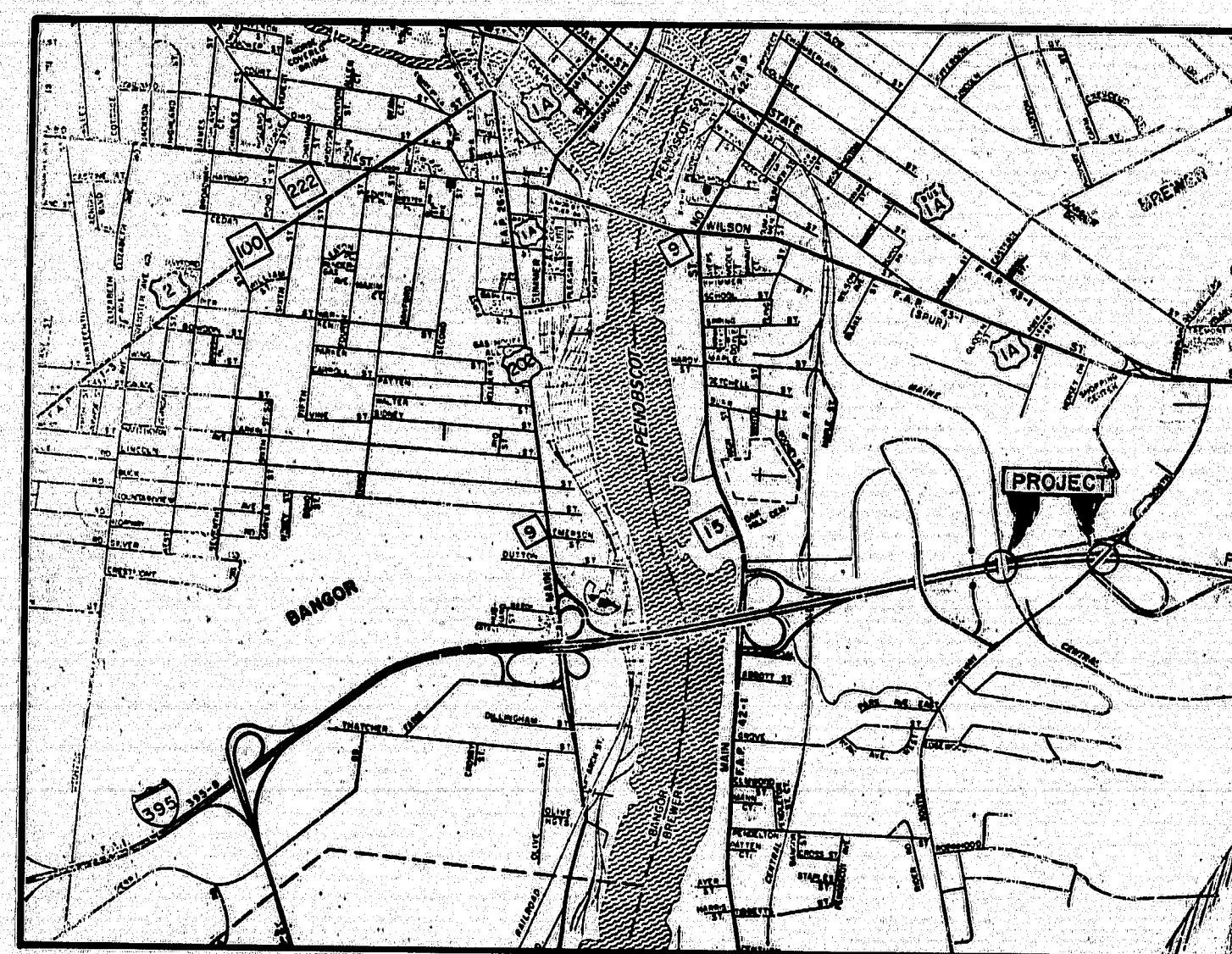


STATE OF MAINE
DEPARTMENT OF TRANSPORTATION



PARKWAY SOUTH
OVER
INTERSTATE 395
AND
MAINE CENTRAL RAILROAD
(CALAIS BRANCH)
OVER
INTERSTATE 395
IN THE CITY OF
BREWER
PENOBSCOT COUNTY



SCALE
MILES 0 0.1 0.2 0.3 0.4 0.5 0.6
LOCATION MAP

PROJECT No. I-IG-395-8(84)176

PROJECT LENGTH
PARKWAY 0.175 MILES
RAILROAD 0.044 MILES
TOTAL LENGTH 0.219 MILES
"AS BUILT" 1985

INTERSTATE 395	
TRAFFIC DATA	
A.D.T. 1983	13,080
A.D.T. 2003	23,600
D.H.V.	3,540
T. (%)	7
D. (%)	55
V.	
P.S.D.	
18 KIPS P 2.5	423

PARKWAY SOUTH	
TRAFFIC DATA	
A.D.T. 1983	5,240
A.D.T. 2003	7,300
D.H.V.	730
T. (%)	7
D. (%)	55
V.	
P.S.D.	
18 KIPS P 2.5	423

NOTE:

All work contemplated under this contract shall be governed by and in conformity with the Standard Specifications (Revision of June 1981) and supplements thereto, except as modified on the plans and in the Special Provisions.

APPROVED:

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
Richard Coleman
CHIEF ENGINEER

DATE
4-15-83

4-15-83

Revised "AS BUILT" TAD 12/11/85

UNITED STATES
DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
REGION 1

APPROVED:

DIVISION ENGINEER DATE

R94-301

F.H.W.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	395-8(84)	1	656

PROJECT DESIGN ENGINEER	DATE
DESIGN - DETAILED	
CHECKED	
REVISIONS	
FIELD CHANGES	
PLANS	

ESTIMATED QUANTITIES					
ITEM NO.	DESCRIPTION	UNIT	Parkway South over I-395		TOTAL
			QUANTITY	QUANTITY	
203.25	Granular Borrow	C.Y.	935	—	935
206.061	Structural Earth Excav.-Drainage & Minor Str., below grade	C.Y.	5	—	5
206.081	Structural Earth Excavation - Abutts. & Ret. walls, etc.	C.Y.	10	220	230
206.091	Structural Rock Excavation - Abutts. & Ret. walls, etc.	C.Y.	5	25	30
206.10	Structural Earth Excavation - Piers	C.Y.	5	—	5
304.10	Aggregate Subbase Course - Gravel	C.Y.	950	170	1120
403.07	Hot Bituminous Pavement, Grading B	Ton	470	—	470
403.08	Hot Bituminous Pavement, Grading C	Ton	200	—	200
403.101	Hot Bit. Pmnt., Grading D Sidewalks, Drives Etc.	Ton	45	—	45
502.2101	Structural Concrete, Abutments & Retaining Walls	C.Y.	315	—	315
502.2102	Structural Concrete, Abutments & Retaining Walls	C.Y.	—	180	180
503.2301	Structural Concrete, Piers - Parkway South	C.Y.	98	—	98
503.2302	Structural Concrete, Piers - M.C.R.R.	C.Y.	—	53	53
502.2601	Str. Conc. Rdwy. & Sdwk. slabs on Stl. Br. - Parkway South	L.S.	1	—	1
502.2602	Str. Conc. Rdwy. & Sdwk. Slabs on Stl. Br. - M.C.R.R.	L.S.	—	1	1
502.310	Structural Concrete Approach Slab	L.S.	1	—	1
503.12	Reinforcing Steel Fab. & Delivered	Lbs.	114,300	28,500	142,800
503.13	Reinforcing Steel Placing	Lbs.	114,300	28,500	142,800
504.7001	Structural Steel Fab. & Delivered - Parkway South	L.S.	1	—	1
504.7002	Structural Steel Fab & Delivered - M.C.R.R.	L.S.	—	1	1
504.7101	Structural Steel Erection - Parkway South	L.S.	1	—	1
504.7102	Structural Steel Erection - M.C.R.R.	L.S.	—	1	1
505.0801	Shear Connectors - Parkway South	L.S.	1	—	1
505.0802	Shear Connectors - M.C.R.R.	L.S.	—	1	1
506.1411	Field Painting New Str. Steel - Parkway South	L.S.	1	—	1
506.1412	Field Painting New Str. Steel - M.C.R.R.	L.S.	—	1	1
507.092	Aluminum Bridge Railing, 2 bar	L.F.	236	—	236
507.094	Aluminum Bridge Railing, 3 bar with poles	L.F.	236	—	236
508.10	Membrane Waterproofing	S.Y.	1200	—	1200
512.08	French Drains	L.F.	205	—	205
514.06	Curing Box for Concrete Cylinders	Each	0.5	0.5	1.0
515.2101	Protective Coating for Conc. Surfaces - Parkway South	L.S.	1	—	1
515.2102	Protective Coating for Conc. Surfaces - M.C.R.R.	L.S.	—	1	1
520.21	Expansion Device - Gland Seal	Each	2	—	2
605.09	6 Inch Underdrain Type B	L.F.	168	—	168
605.095	6 Inch Underdrain Type B (Pipe Only)	L.F.	—	90	90
606.17	Guard Rail Type 3b - Single Rail	L.F.	495	—	495
606.22	Guard Rail Type 3b - over 15 foot radius	L.F.	13	—	13
606.265	Terminal End - Single Rail - Galvanized Steel	Each	3	—	3
606.35	Guard Rail Delineator Post	Each	1	—	1
609.11	Vertical Curb Type 1	L.F.	920	—	920
609.13	Vertical Bridge Curb Type 1	L.F.	476	—	476
615.07	Loom	C.Y.	15	—	15
616.08	Sodding	S.Y.	105	—	105
618.14	Seeding Method Number 2	UNIT	2	—	2

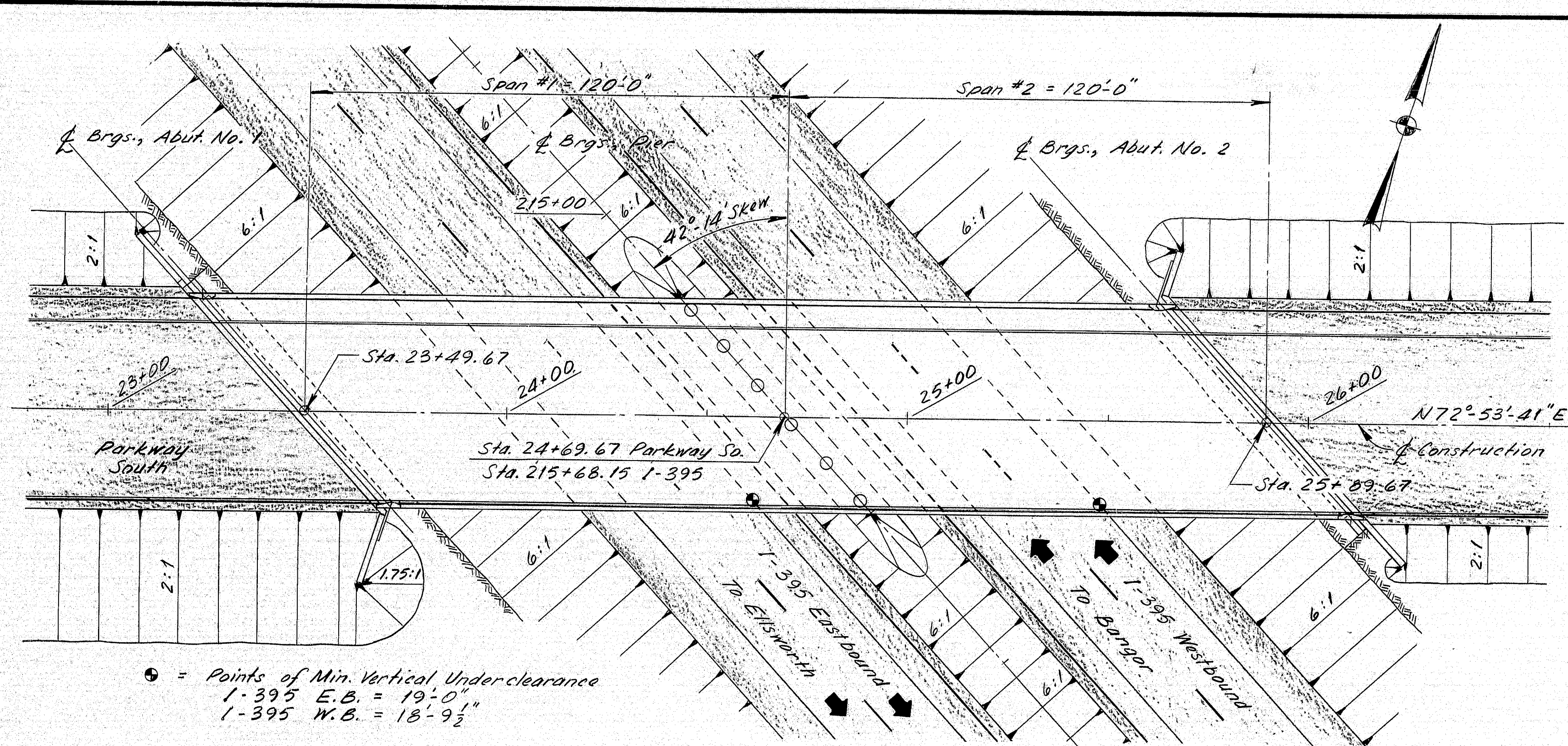
ESTIMATED QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	Parkway South over I-395		TOTAL
			QUANTITY	QUANTITY	
619.12	Mulch	UNIT	2	—	2
627.68	Temporary 4" Painted Pmnt. Marking Line, Yellow or White	L.F.	1850	—	1850
629.05	Hand Labor, Straight Time	M.H.	10	10	20
631.10	Air Compressor (including operator)	M.H.	10	10	20
631.11	Air Tool (including operator)	H.R.	10	10	20
631.12	All Purpose Excavator (including operator)	H.R.	10	10	20
631.171	Truck Small (including operator)	H.R.	10	10	20
631.22	Front End Loader (including operator)	H.R.	10	10	20
639.19	Field Office Type B	Each	0.5	0.5	1.0
652.31	Type 1 Barricades	Each	6	—	6
652.34	Cones	Each	10	—	10
652.35	Construction Signs	S.F.	100	—	100
652.36	Maintenance of Traffic Control Signs	C.D.	20	—	20
659.10	Mobilization	L.S.	0.5	0.5	1.0
660.20	On-the-Job Training	M.H.	500	500	1000
ESTIMATE OF LUMP SUM ITEMS					
502.2601	Str. Concrete Roadway & Sidewalk Slabs on Steel Bridges	C.Y.	480	—	480
502.2602	Str. Concrete Roadway & Sidewalk Slabs on Steel Bridges	C.Y.	—	75	75
502.310	Structural Concrete Approach Slabs	C.Y.	33	—	33
504.7001	Structural Steel Fab. & Delivered	Lbs.	401,000	—	401,000
504.7002	Structural Steel Fab & Delivered	Lbs.	—	407,000	407,000
504.7101	Structural Steel Erection	Lbs.	401,000	—	401,000
504.7102	Structural Steel Erection	Lbs.	—	407,000	407,000
505.8001	Shear Connectors	Each	2496	—	2496
505.8002	Shear Connectors	Each	—	548	548
506.1411	Field Painting New Structural Steel	Lbs.	401,000	—	401,000
506.1412	Field Painting New Structural Steel	Lbs.	—	407,000	407,000

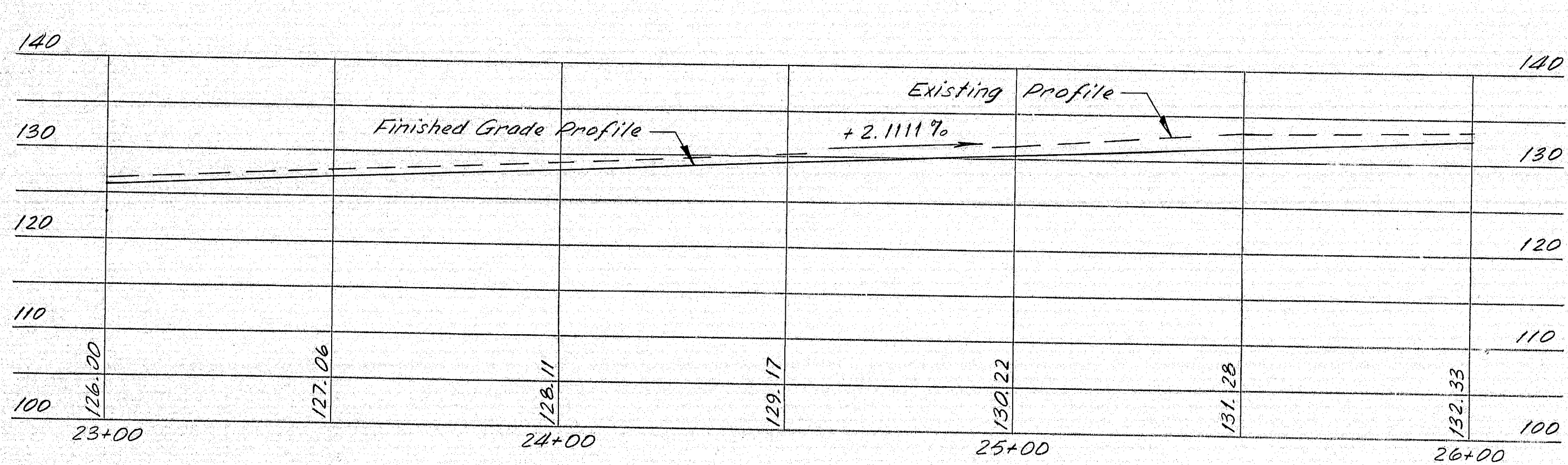
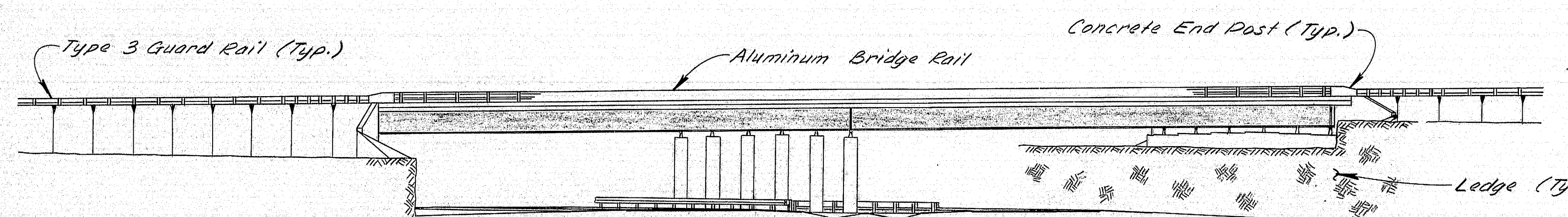
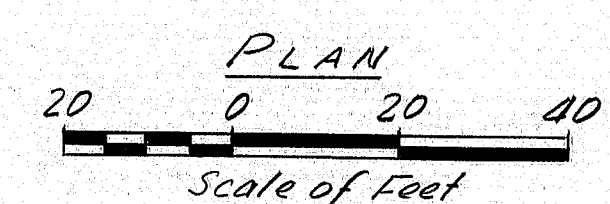
STATE OF MAINE DEPARTMENT OF TRANSPORTATION
BREWER
PARKWAY SOUTH OVER I-395 and M.C.R.R. OVER I-395
ESTIMATED QUANTITIES
AUGUSTA, MAINE April 1983

R94-302

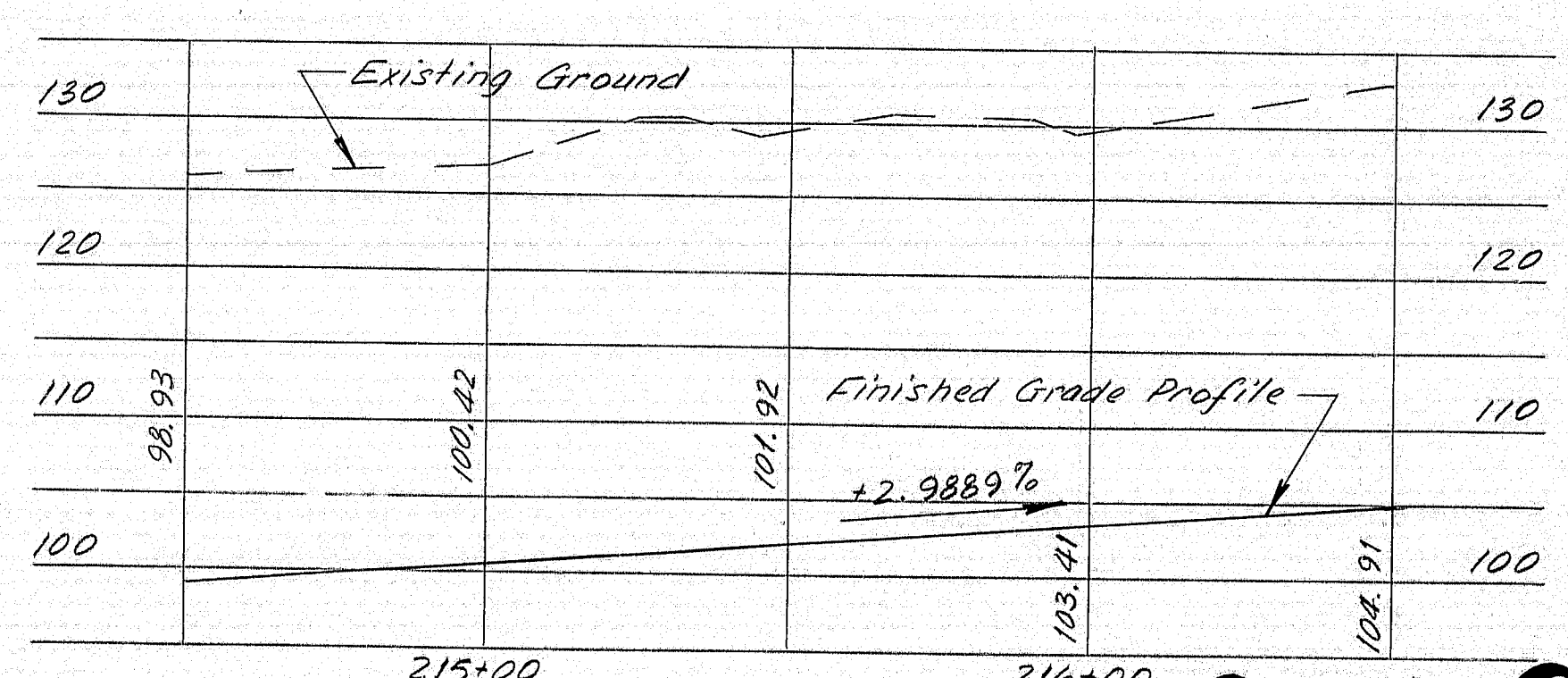
F.R.W.A. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-16-395-8(84)176	3	65



• = Points of Min. Vertical Underclearance
 I-395 E.B. = 19'-0"
 I-395 W.B. = 18'-9"



PROFILE - PARKWAY SOUTH



PROFILE - INTERSTATE 395

R94-303

SPECIFICATIONS

DESIGN: Load Factor Design per AASHTO Standard Specifications for Highway Bridges 1977 and interim specifications thru 1982.

CONTRACT: State of Maine, Department of Transportation, Standard Specifications, Highways and Bridges, Revision of June 1981.

DESIGN LOADING

LIVE LOAD: HS 25

Stress Cycles: 500,000

MATERIALS

CONCRETE: Class "A"

REINFORCING STEEL: ASTM A615, Grade 60

STRUCTURAL STEEL: All material (Except as otherwise noted) - ASTM A36

High Strength Bolts: ASTM A325, Type 1

BASIC DESIGN STRESSES

CONCRETE: $f'_c = 3000$ psi

REINFORCING STEEL: $F_y = 60,000$ psi

STRUCTURAL STEEL: ASTM A572 $F_y = 50,000$ psi

ASTM A36 $F_y = 36,000$ psi

ASTM A325 $f_u = 33,000$ psi

TRAFFIC DATA (PARKWAY SO.)

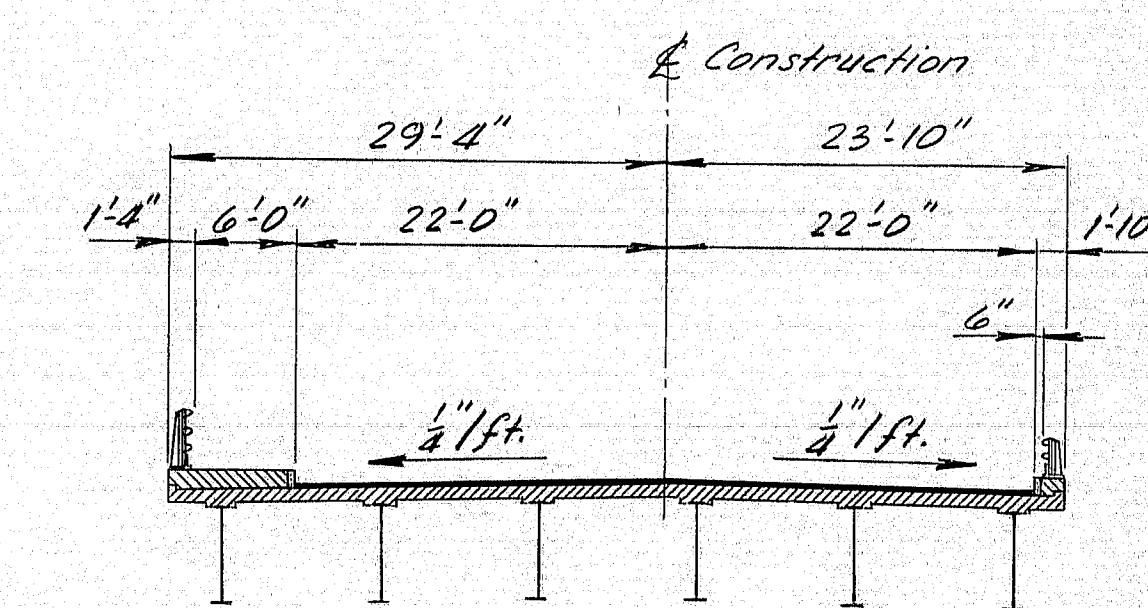
AADT 1983	5240
AADT 2003	7300
DHV	730
T (%)	7
D (%)	55
18 Kip P2.5	172

TRAFFIC DATA (INTERSTATE 395)

AADT 1983	13,080
AADT 2003	23,600
DHV	3540
T (%)	7
D (%)	55
18 Kip P2.5	423

INDEX OF SHEETS

General Plan	1
Survey Plan and Profile	2
Abutment No. 1	3
Abutment No. 1 Footing Plan and Sections	4
Abutment No. 2	5
Abutment No. 2 Sections	6
Pier	7
Structural Steel	8
Structural Steel	9
Superstructure	10
Reinforcing Steel Schedule	11
Reinforcing Steel Schedule	12
Typical Sections	13
Plan and Profile	14-15
Cross Sections	16-20
Foundation Survey	21
Boring Details	22



TYPICAL BRIDGE SECTION

STANDARD DETAILS

BRIDGE:	HIGHWAY:
BD 100-81	Nov. 1980 (4)
BD 101-81	Nov. 1980 (5)
BD 113-81	Nov. 1980 (6)
BD 114-81	Nov. 1980 (6)
BD 115-81	
BD 116-81	
BD 120-81	
BD 125-82	
BD 126-81	
BD 127-81	

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

PARKWAY SOUTH
OVER
INTERSTATE 395

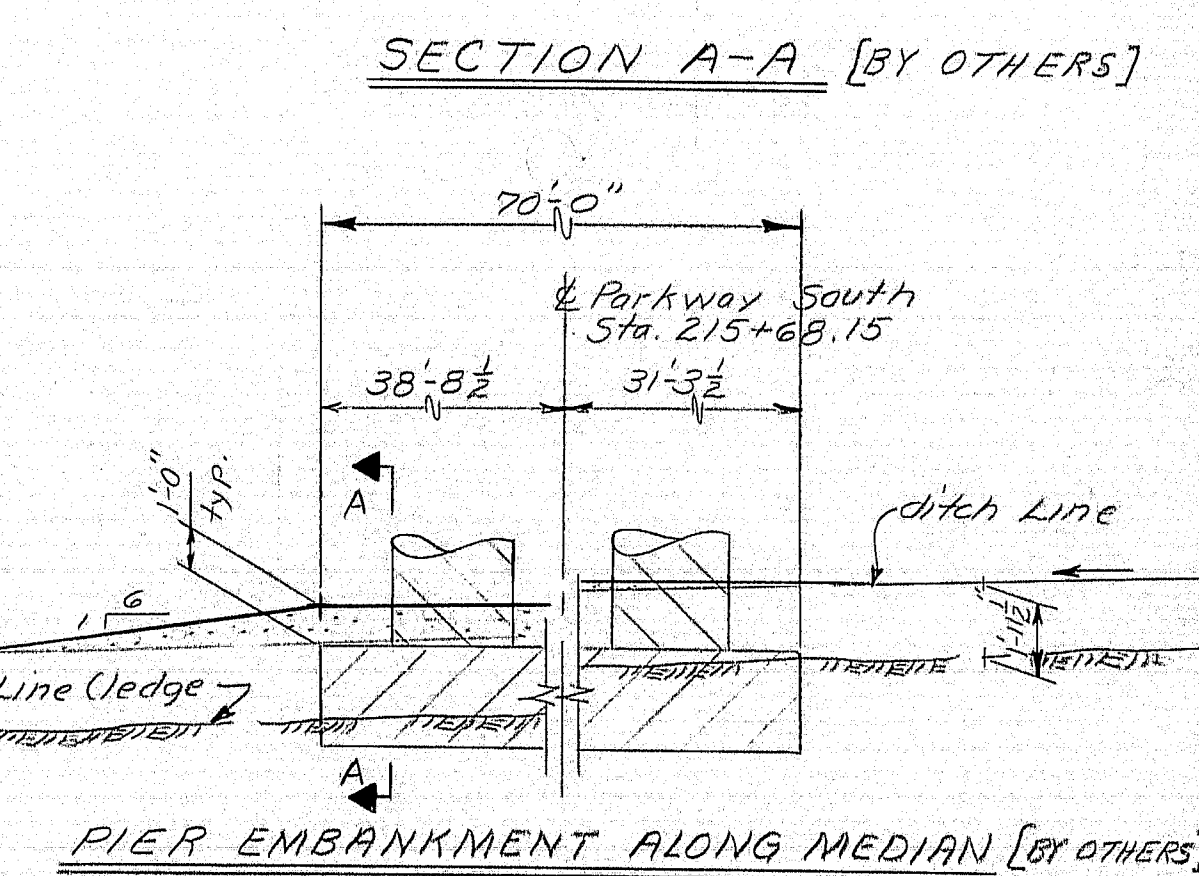
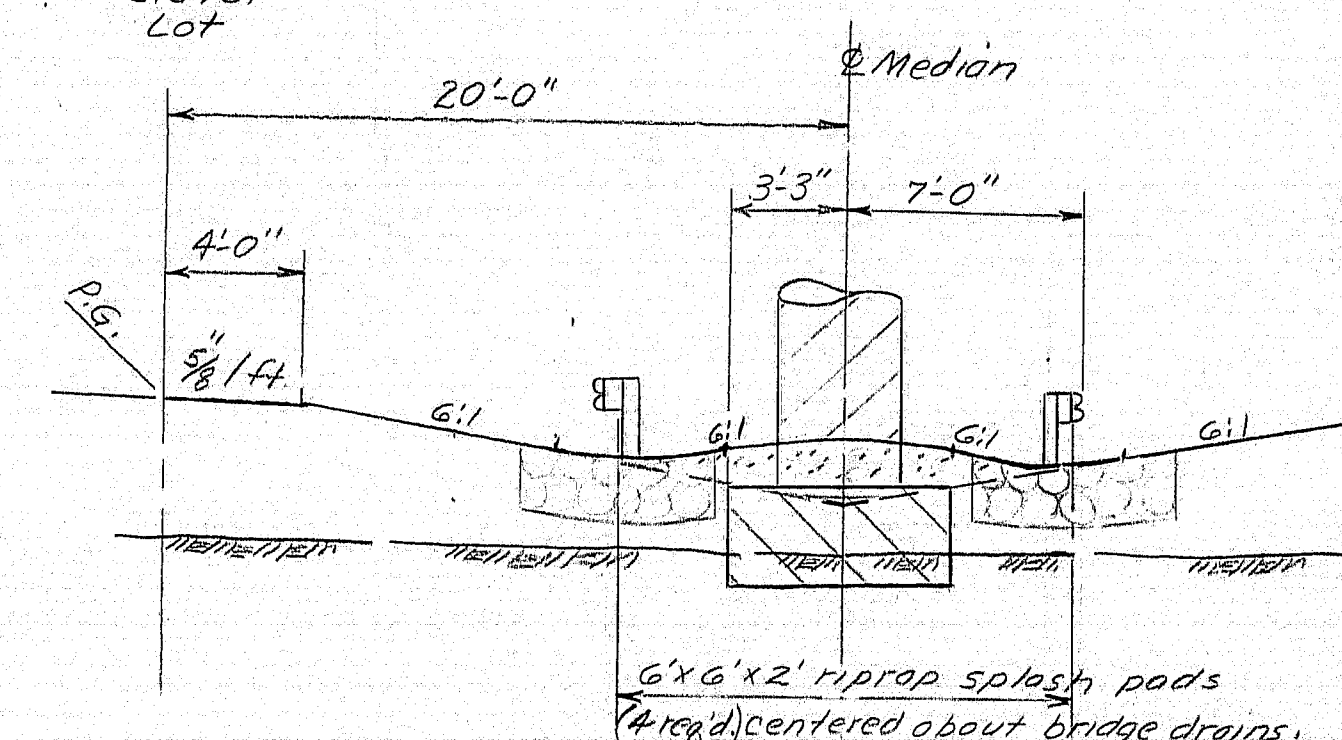
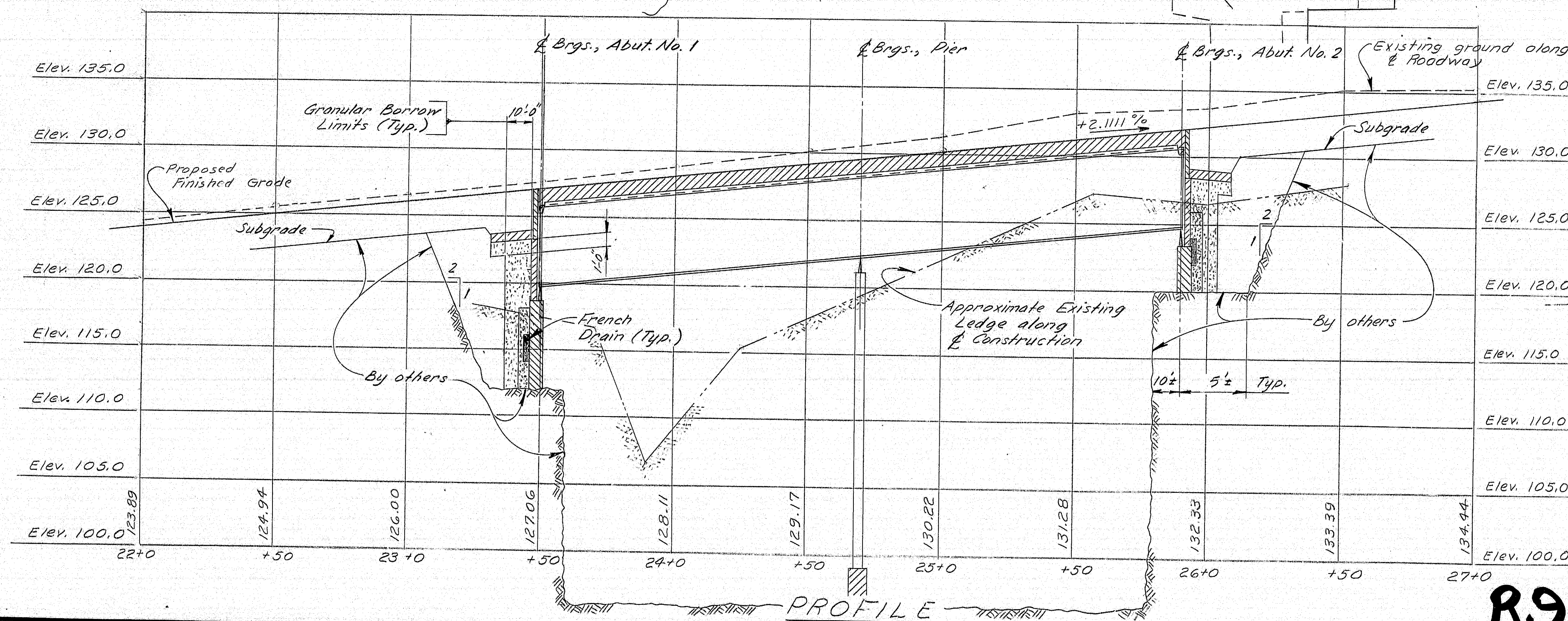
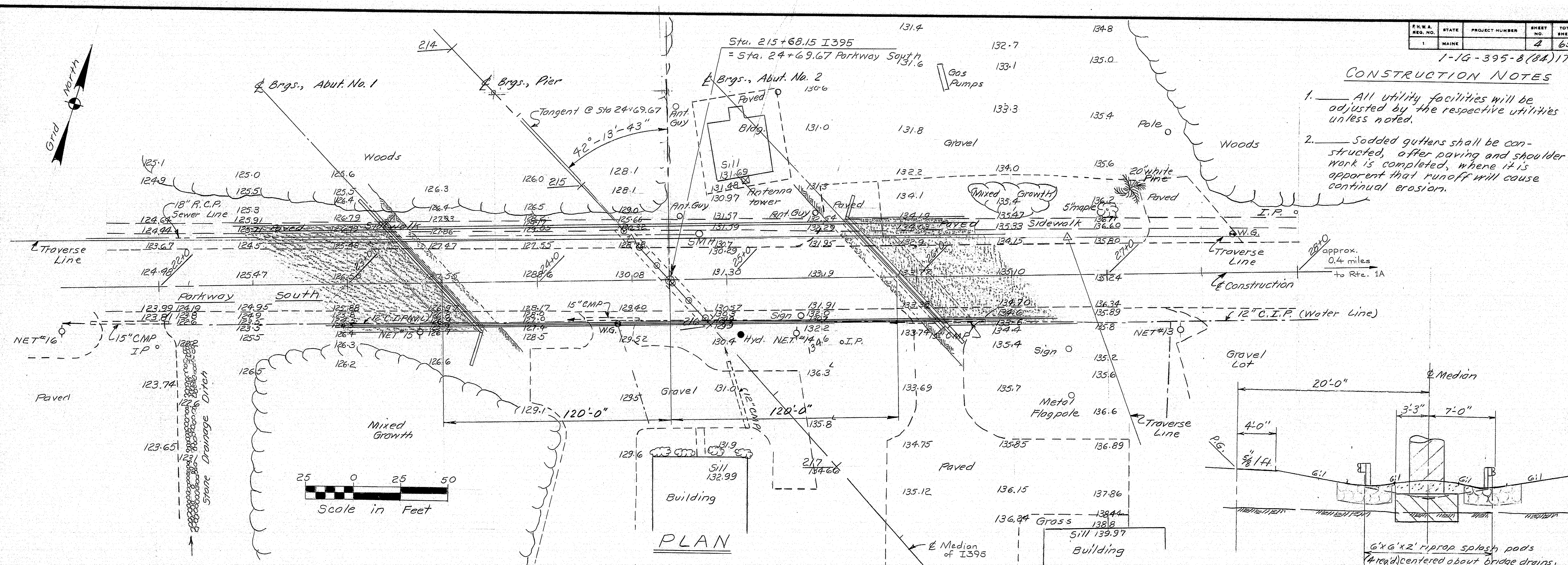
BREWER
PENOBSCOT COUNTY
GENERAL PLAN

SHEET 1 OF 22 AUGUSTA, MAINE April, 1983

F.H.W.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOT. SHEETS
1	MAINE		4	65

CONSTRUCTION NOTES

1. _____ All utility facilities will be adjusted by the respective utilities unless noted.
2. _____ Sodded gutters shall be constructed, after paving and shoulder work is completed, where it is apparent that runoff will cause continual erosion.



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

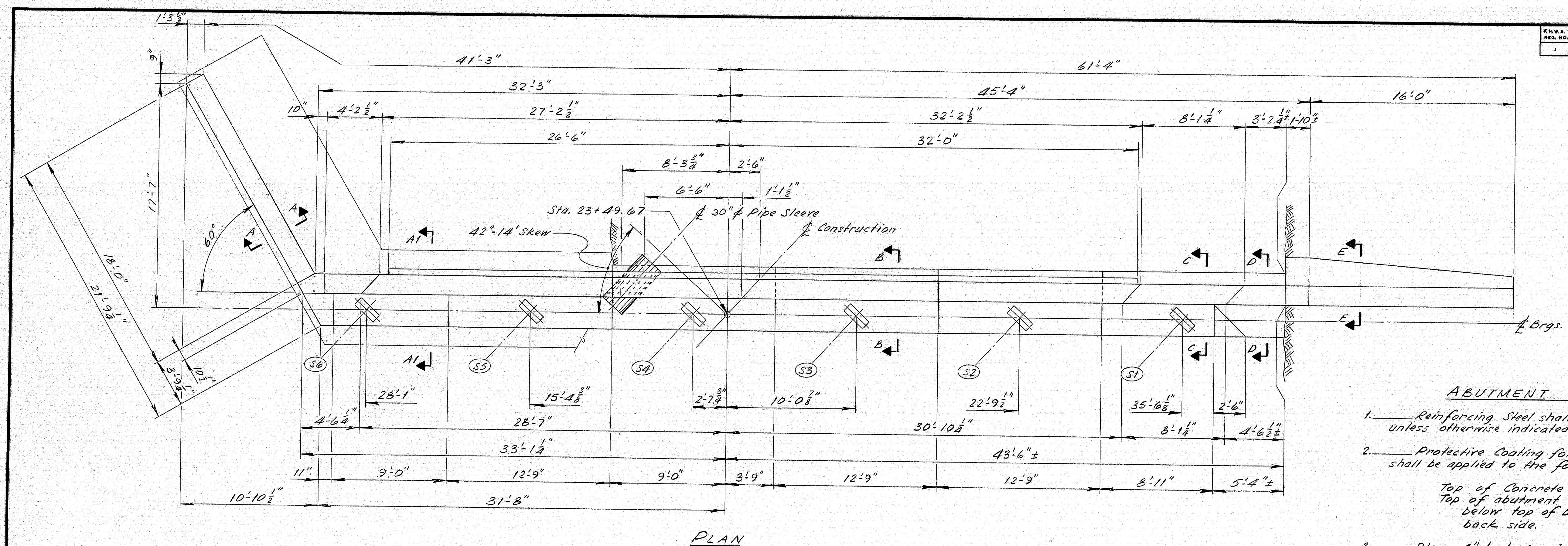
PARKWAY SOUTH
OVER
INTERSTATE 395

BREWER
PENOBSCOT COUNTY
SURVEY PLAN AND PROFILE

SHEET 2 OF 22 AUGUSTA, MAINE April, 1983

R94-304

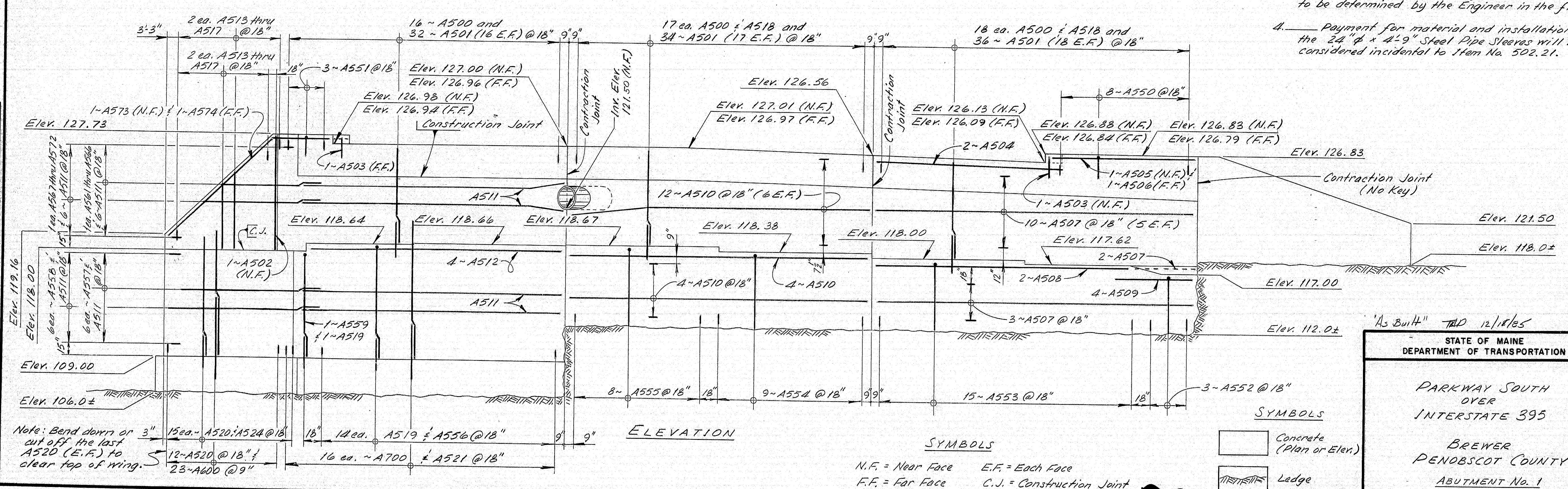
REV. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-1G-395-8(8A)176	5	65



PLAN

ABUTMENT NOTES

1. Reinforcing Steel shall have 2" min. cover unless otherwise indicated.
2. Protective Coating for Concrete Surfaces shall be applied to the following areas:
Top of Concrete Curbs
Top of abutment backwalls and 1'-0" below top of backwalls on the back side.
3. Place 4" dia drains in breastwall and wings at 20'-0" maximum spacing. Exact locations to be determined by the Engineer in the field.
4. Payment for material and installation of the 24" dia x 4'-9" Steel Pipe Sleeves will be considered incidental to Item No. 502.21.



ELEVATION

SYMBOLS

N.F. = Near Face E.F. = Each Face
F.F. = Far Face C.J. = Construction Joint

SYMBOLS

Concrete (Plan or Elevation)
Ledge

As Built TAD 12/18/85

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

PARKWAY SOUTH
OVER
INTERSTATE 395

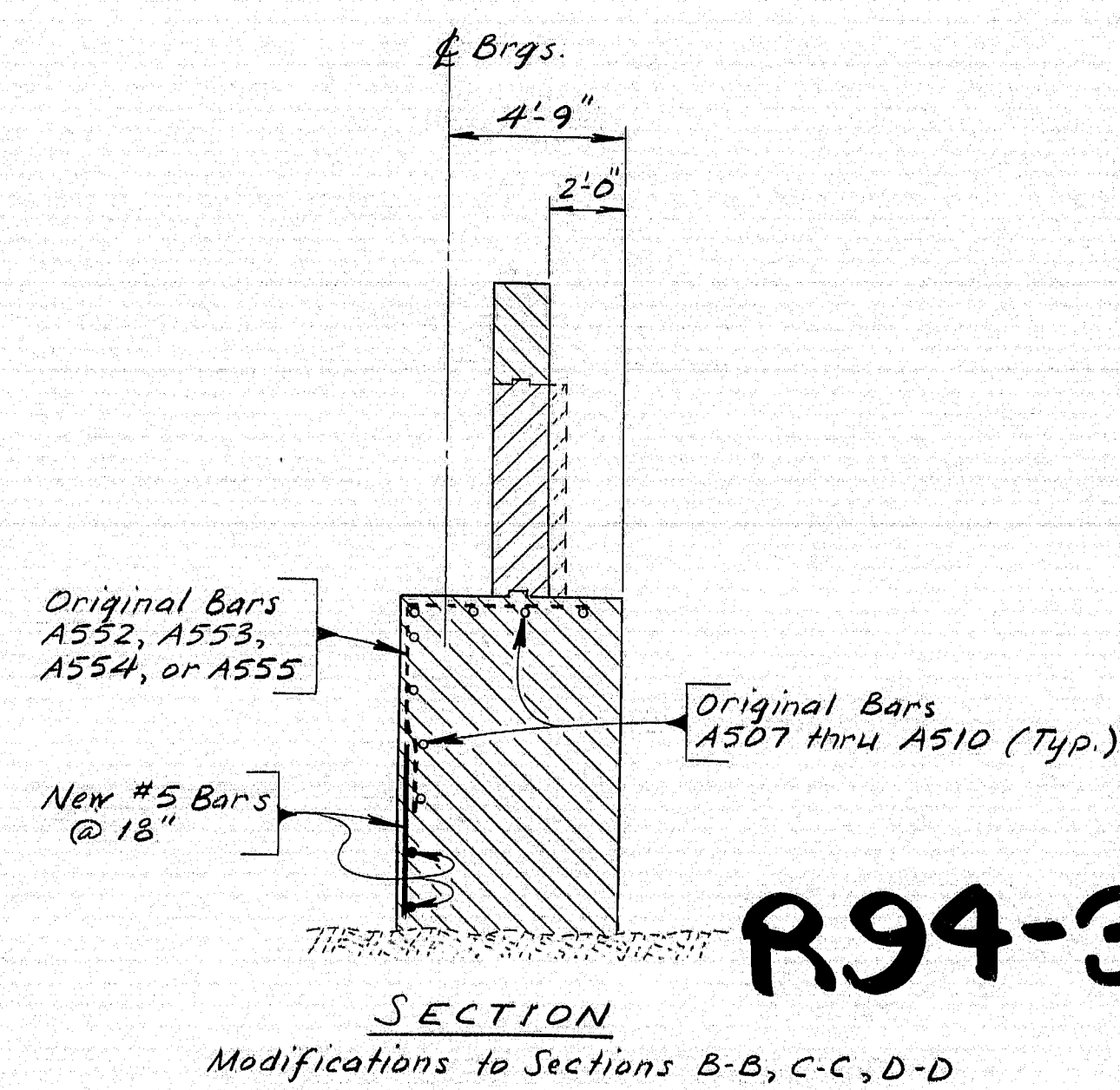
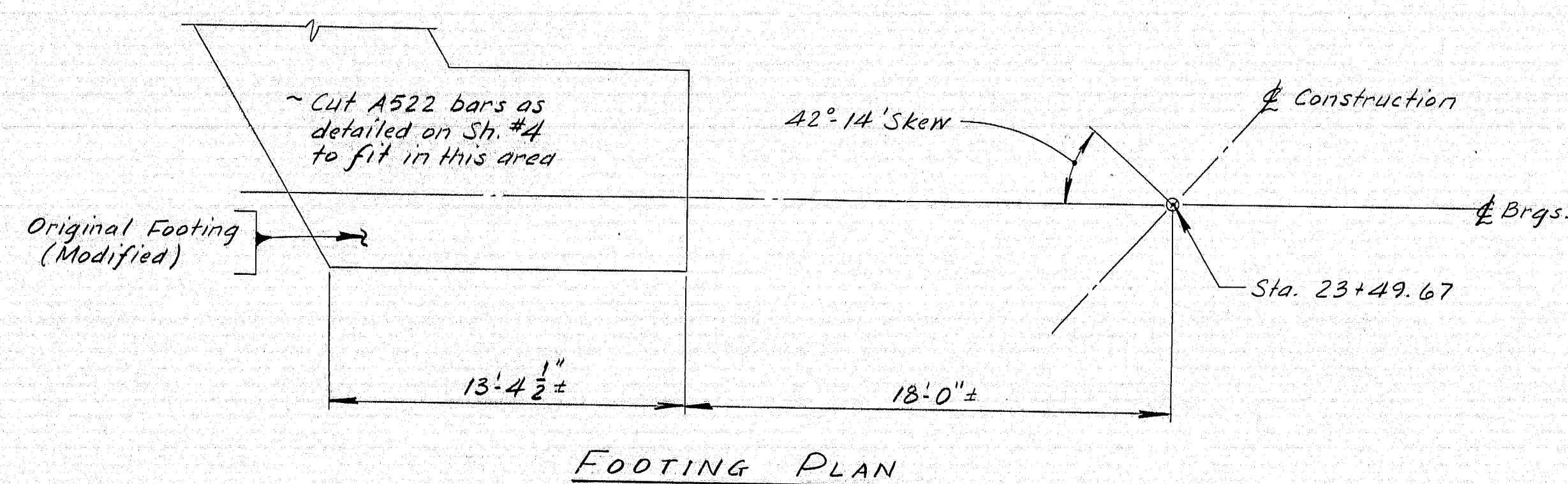
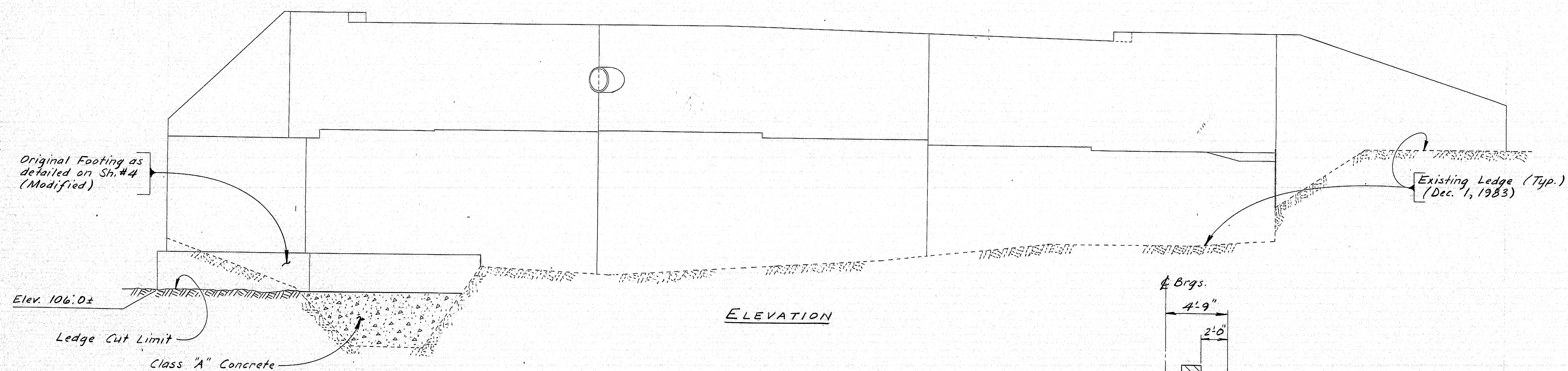
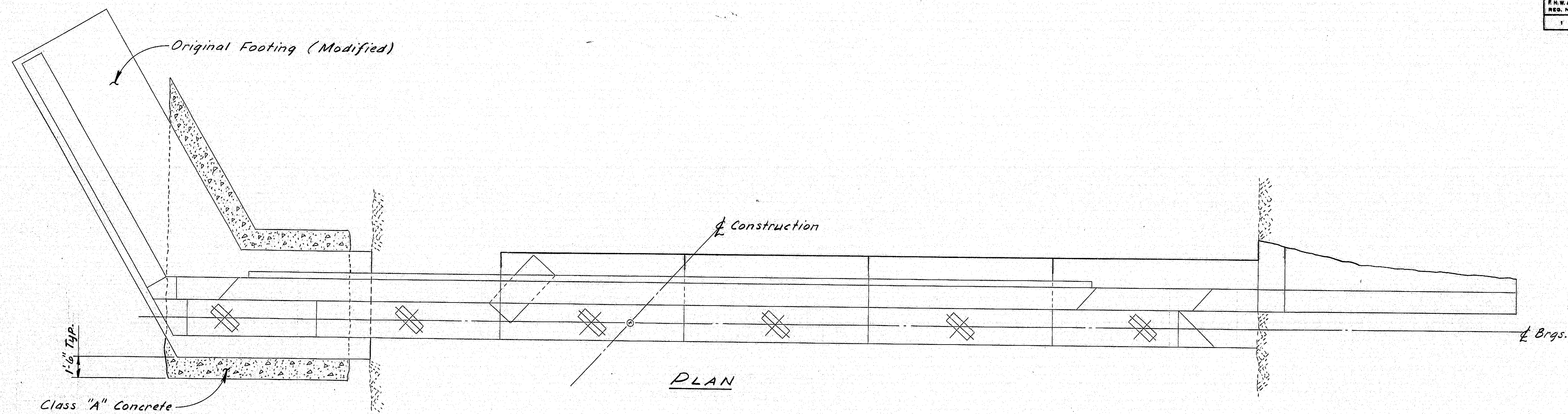
BREWER
PENOBSCOT COUNTY

ABUTMENT No. 1

SHEET 3 OF 22 AUGUSTA, MAINE April, 1983

R94-305

F.H.W.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-1G-395-8(84)176	5A 65	



"As Built" TAD 12/11/85
For dimensions and details not shown, see Sh. No's 3 & 4

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

PARKWAY SOUTH
OVER
INTERSTATE 395
BREWER
PENOBSCOT COUNTY
ABUT. No. 1 MODIFICATIONS

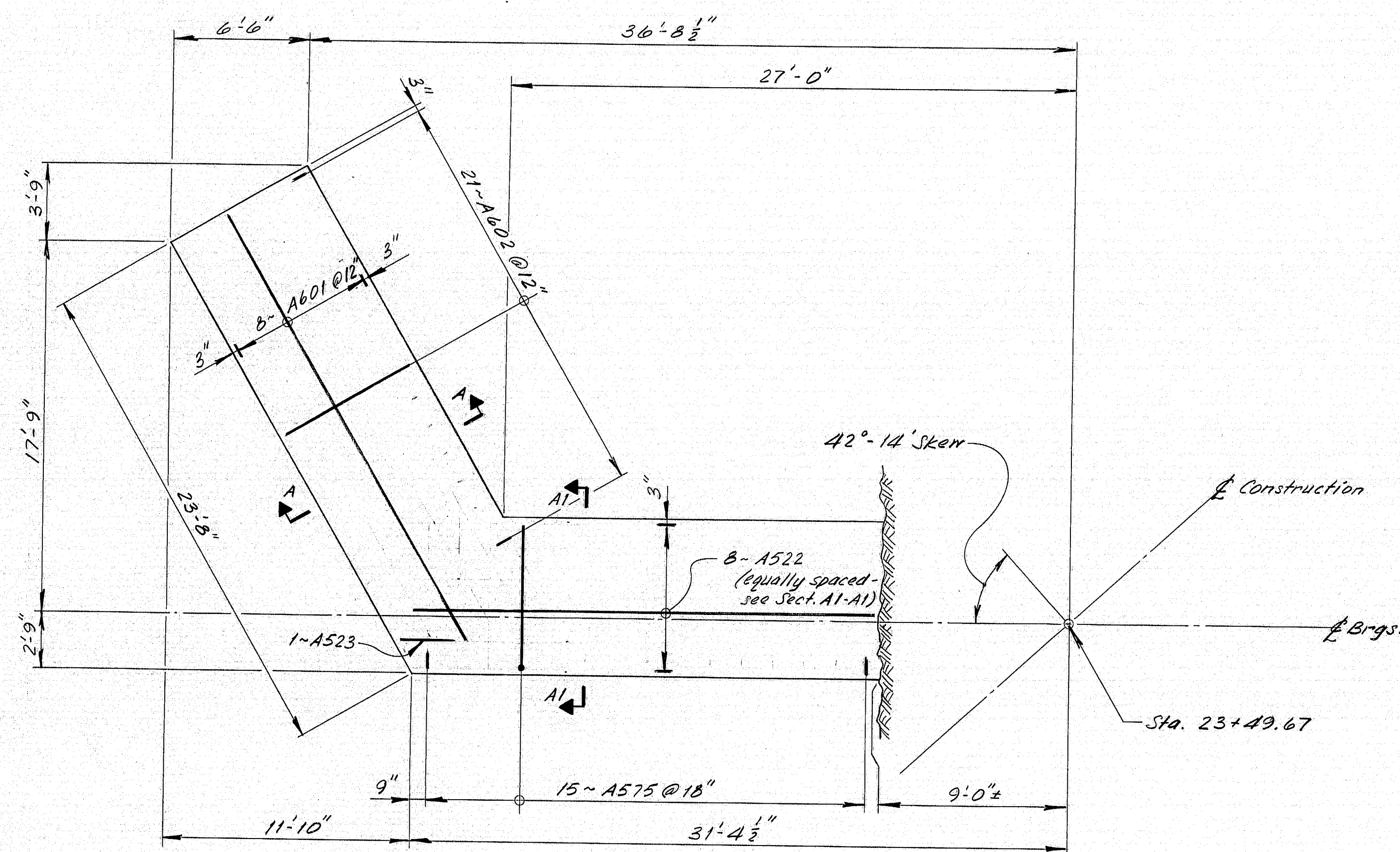
SHEET 3A OF 22 AUGUSTA, MAINE Dec. 1983

R94-306

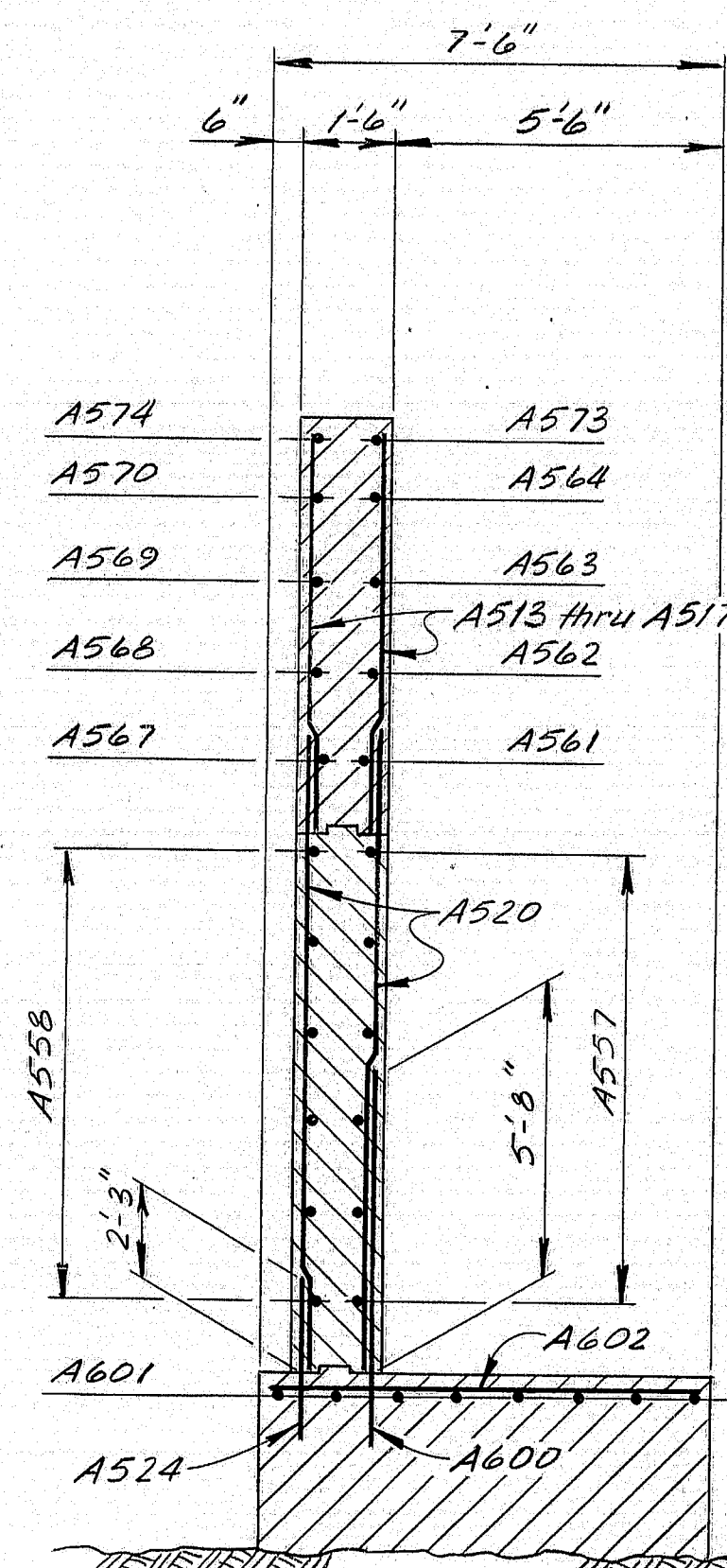
PROJECT DESIGN ENGINEER	BY	DATE
DESIGN - DETAILED	JES	D. Dorman Dec. '83
CHECKED	KSB	
REVISIONS		
FIELD CHANGES		

BRIDGE 44122-25101

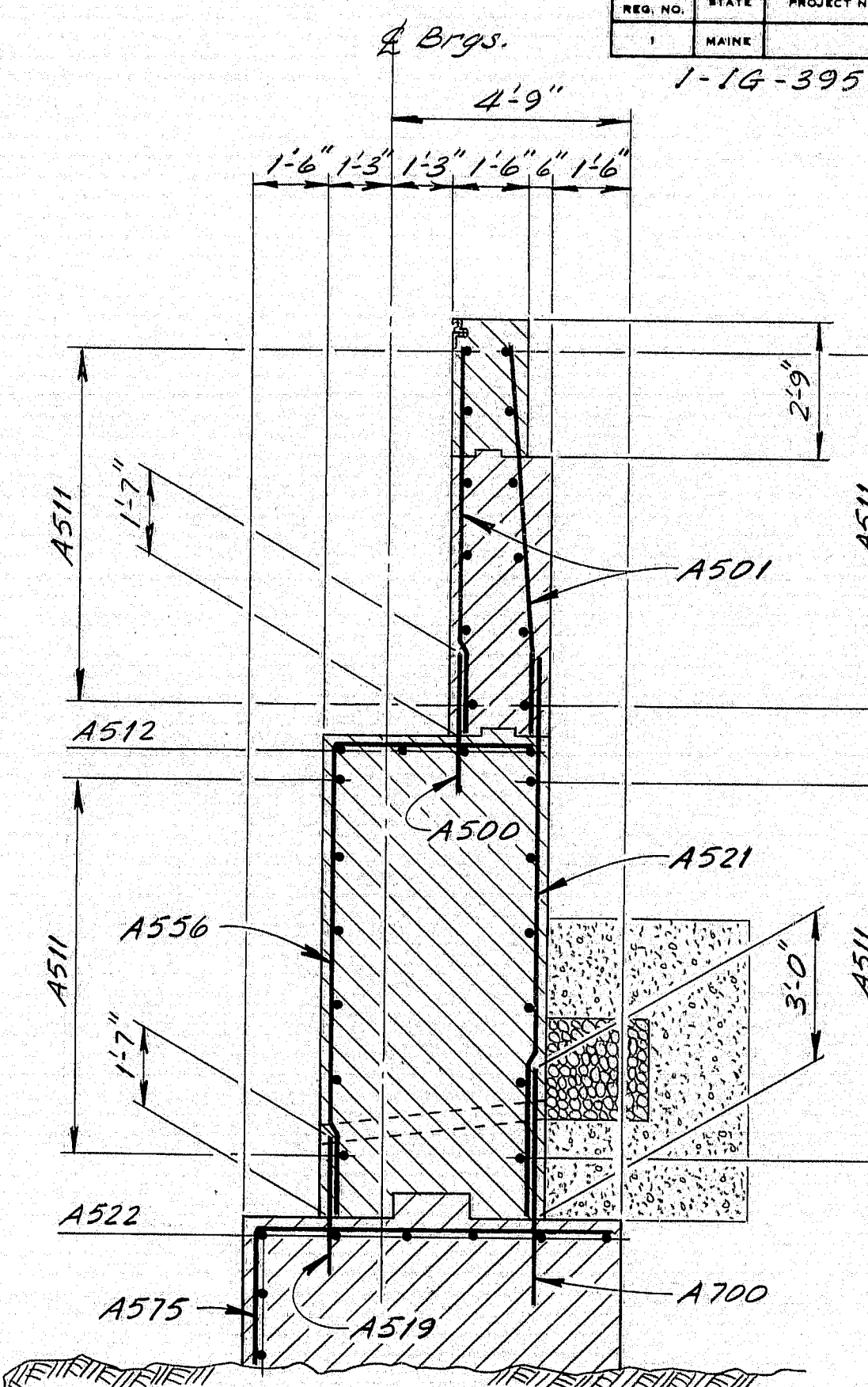
F.R.W.A. SHEET NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-1G-395-8(84)176	6	65



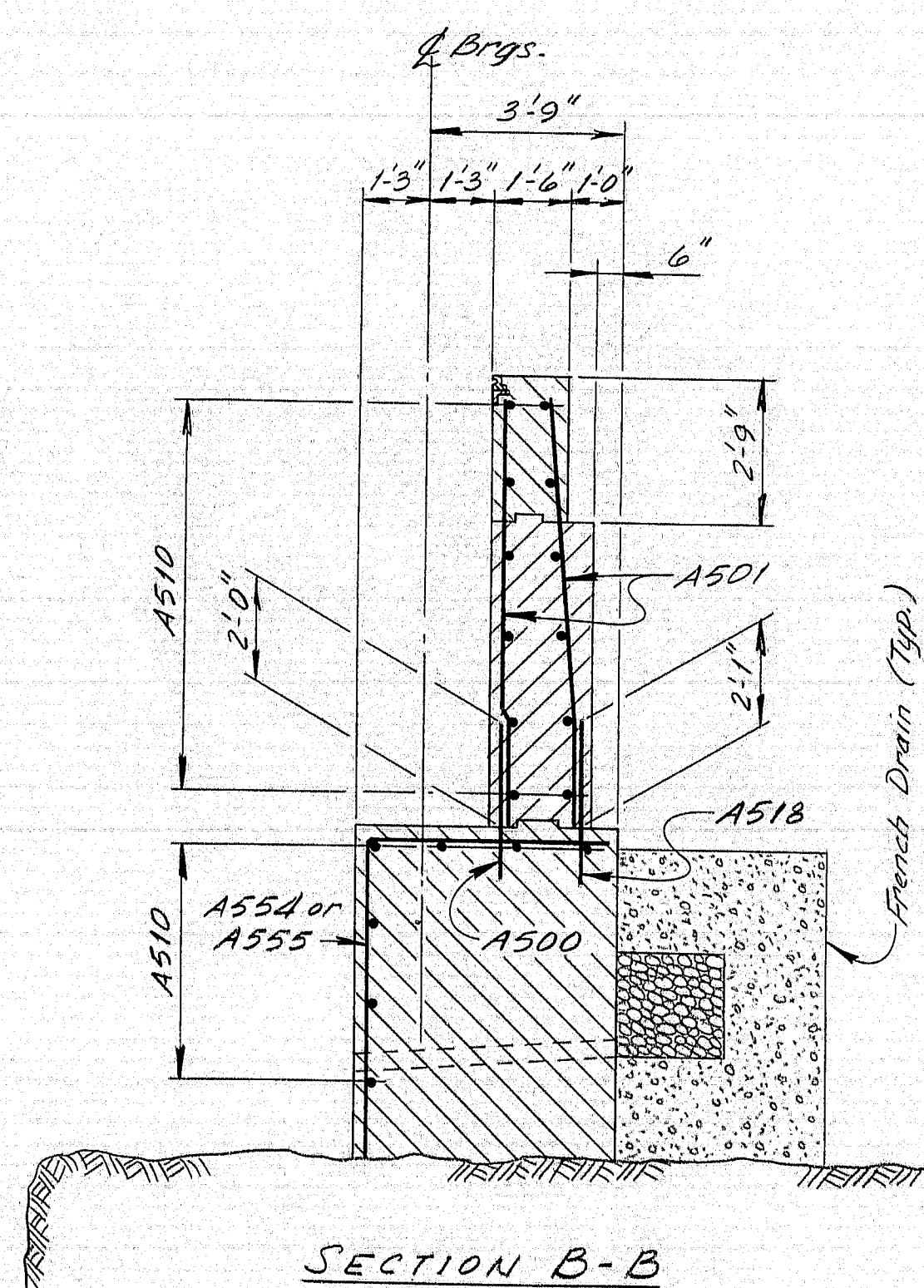
FOOTING PLAN



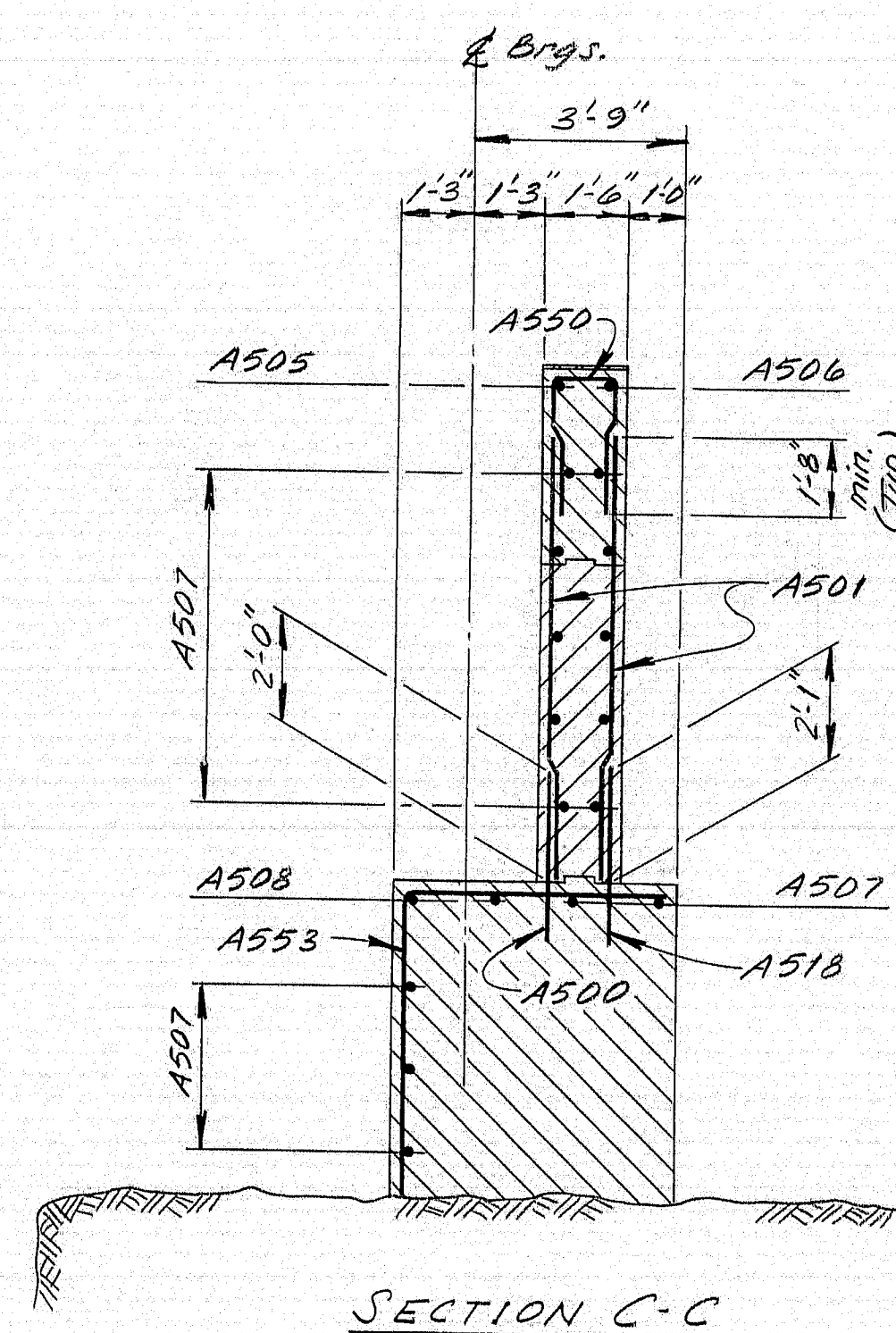
SECTION A-A



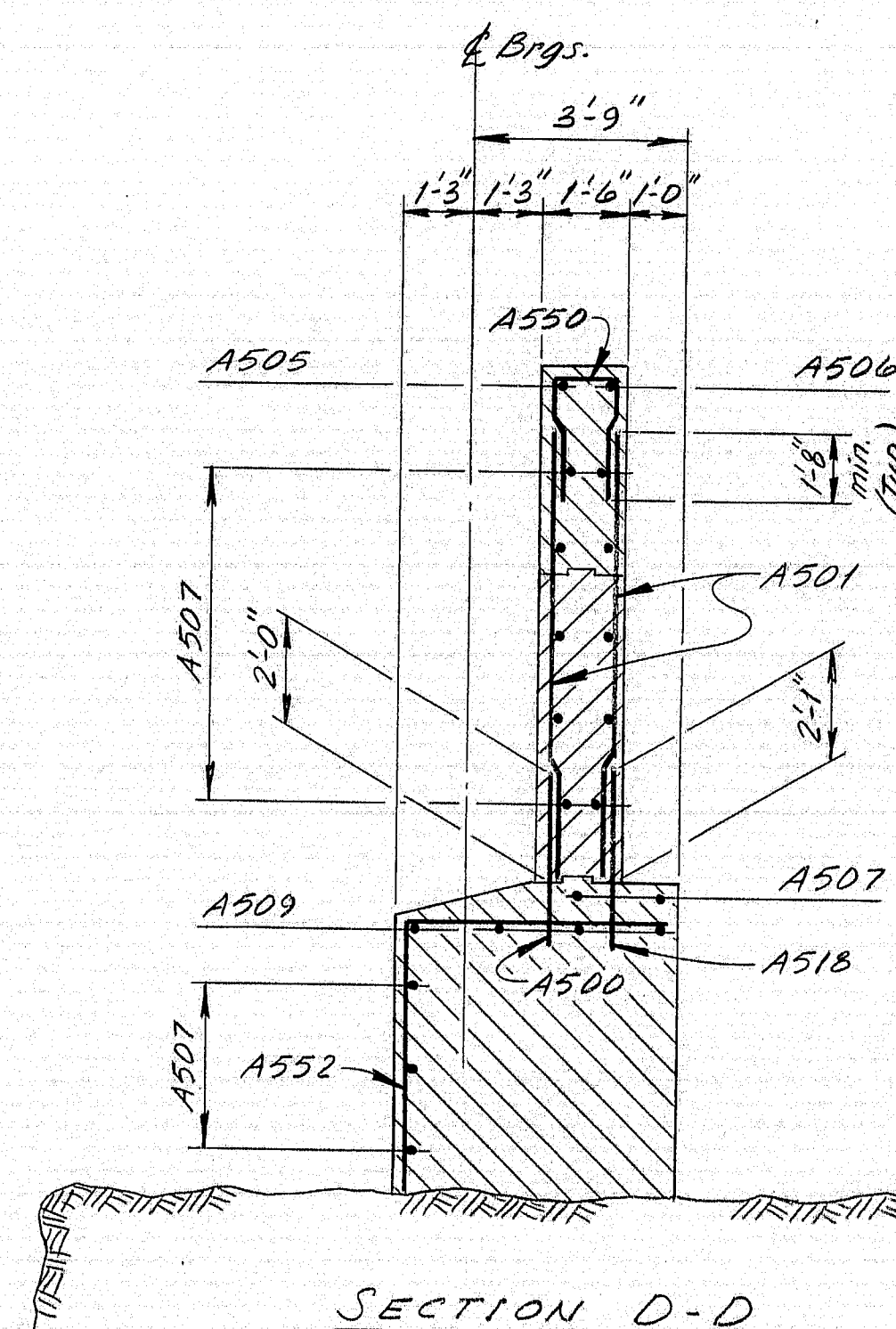
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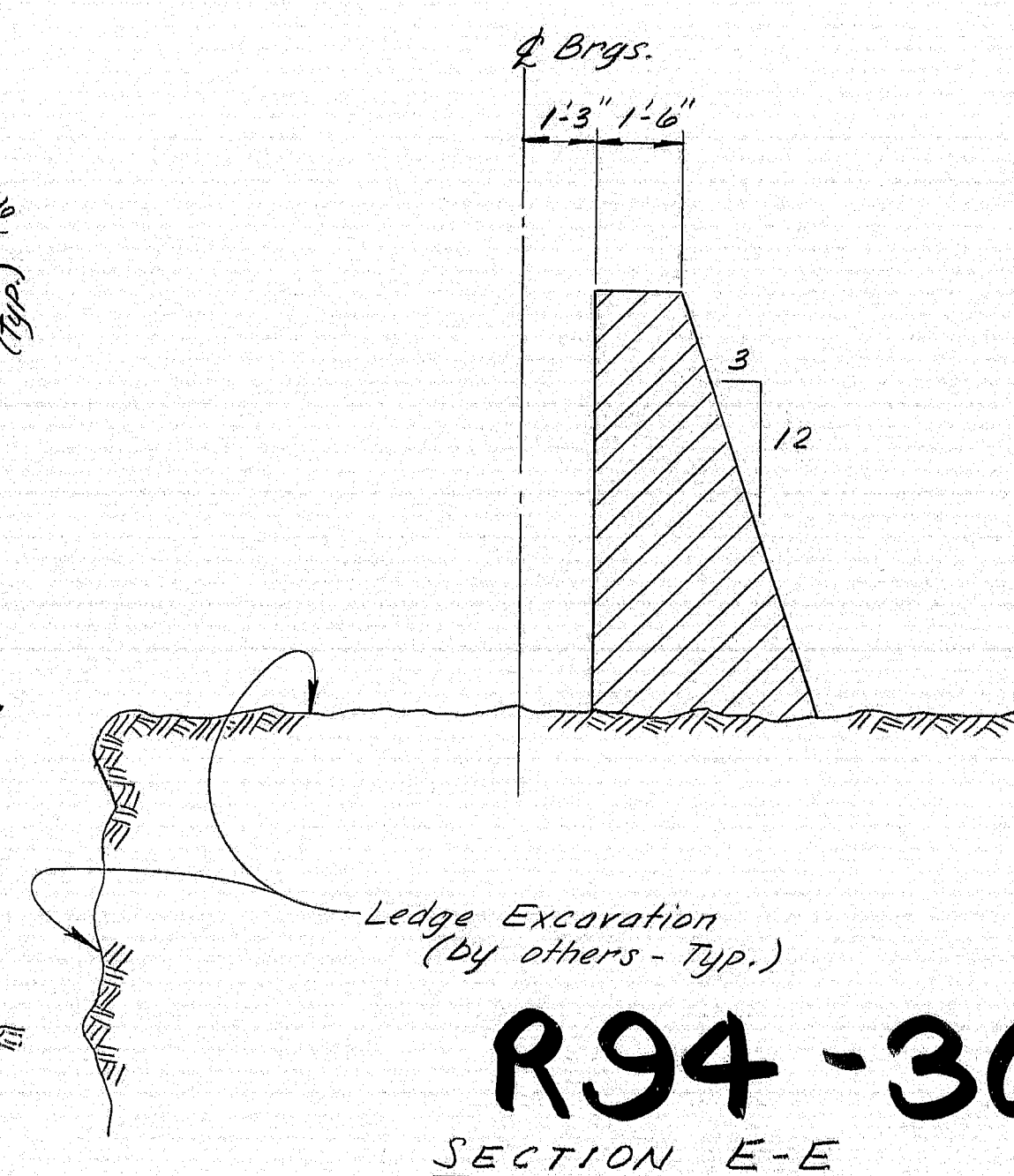
SECTION B-B



SECTION C-C



SECTION D-D



SECTION E-E

SYMBOLS

- Concrete (Plan or Elev.)
- Concrete (Section)
- Ledge

'As Built" TAD 12/11/85

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

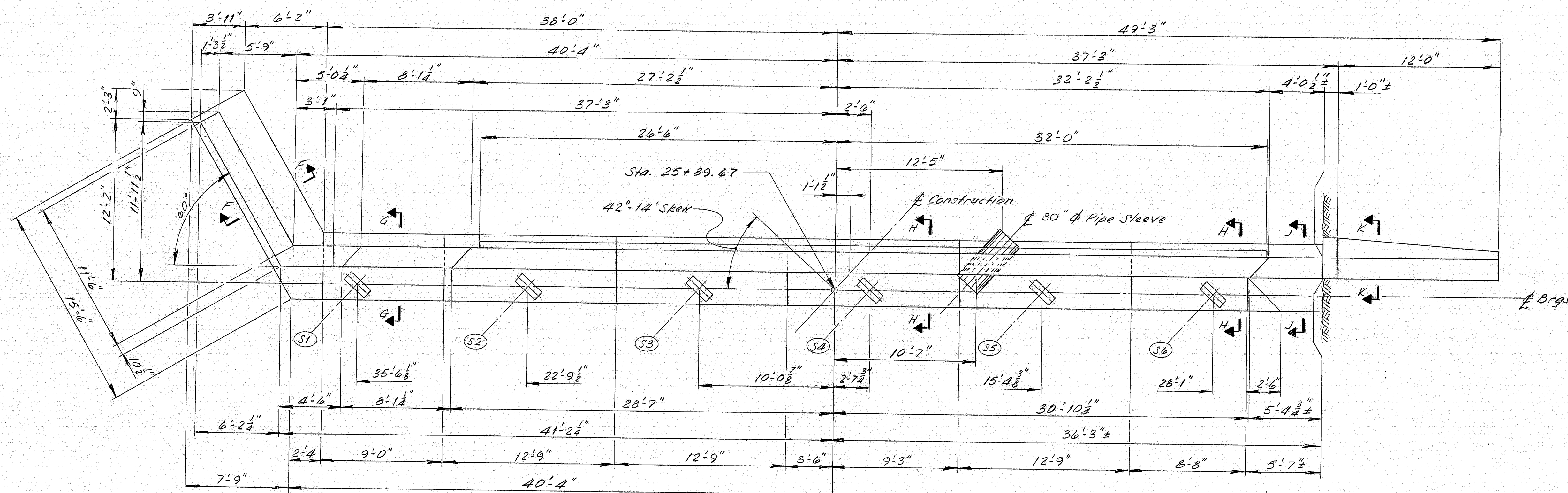
PARKWAY SOUTH
OVER
INTERSTATE 395
BREWER
PENOBSCOT COUNTY
ABUTMENT No. 1
FOOTING PLAN AND SECTIONS
SHEET 4 OF 22 AUGUSTA, MAINE April 1983

R94-307

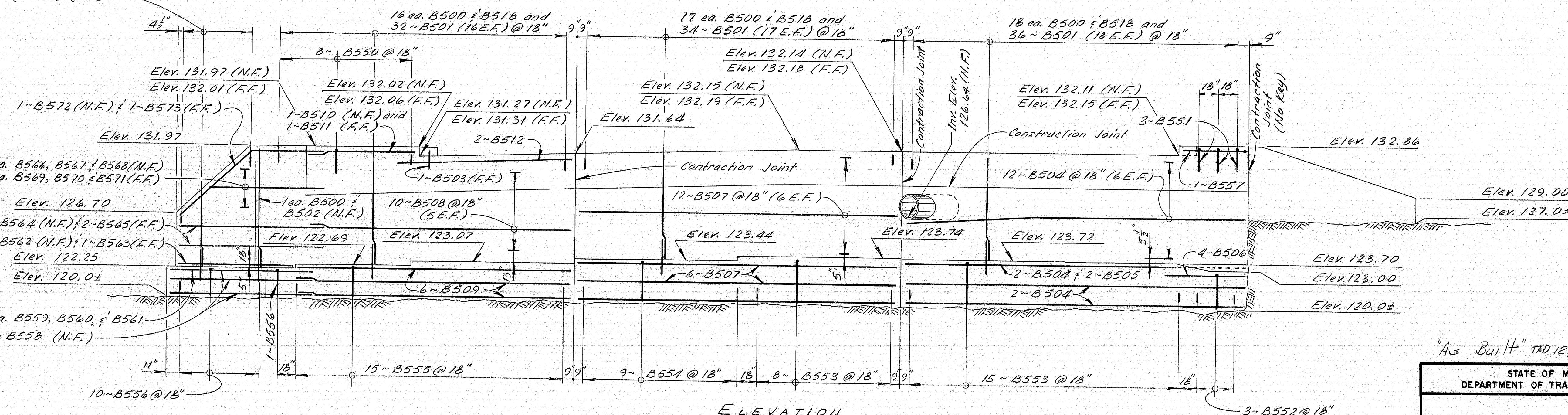
PROJECT DESIGN ENGINEER	BY	DATE
DESIGN - DETAILED	U. Brown	10/20/82
CHECKED	R. B. B.	April 83
REVISIONS		
FIELD CHANGES		

BRUNING 44-132-45710

F.R.A. REV. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-14-395-8(84)176	7	65



8 ea. B517, B519 and
4 ea. B513 thru B516
(2 E.F.) @ 18"



"As Built" 12/11/85

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

PARKWAY SOUTH
OVER
INTERSTATE 395

BREWER
PENOBSCOT COUNTY
ABUTMENT No. 2

SHEET 5 OF 22 AUGUSTA, MAINE April, 1993

LEGEND

N.F. = Near Face
F.F. = Far Face
E.F. = Each Face

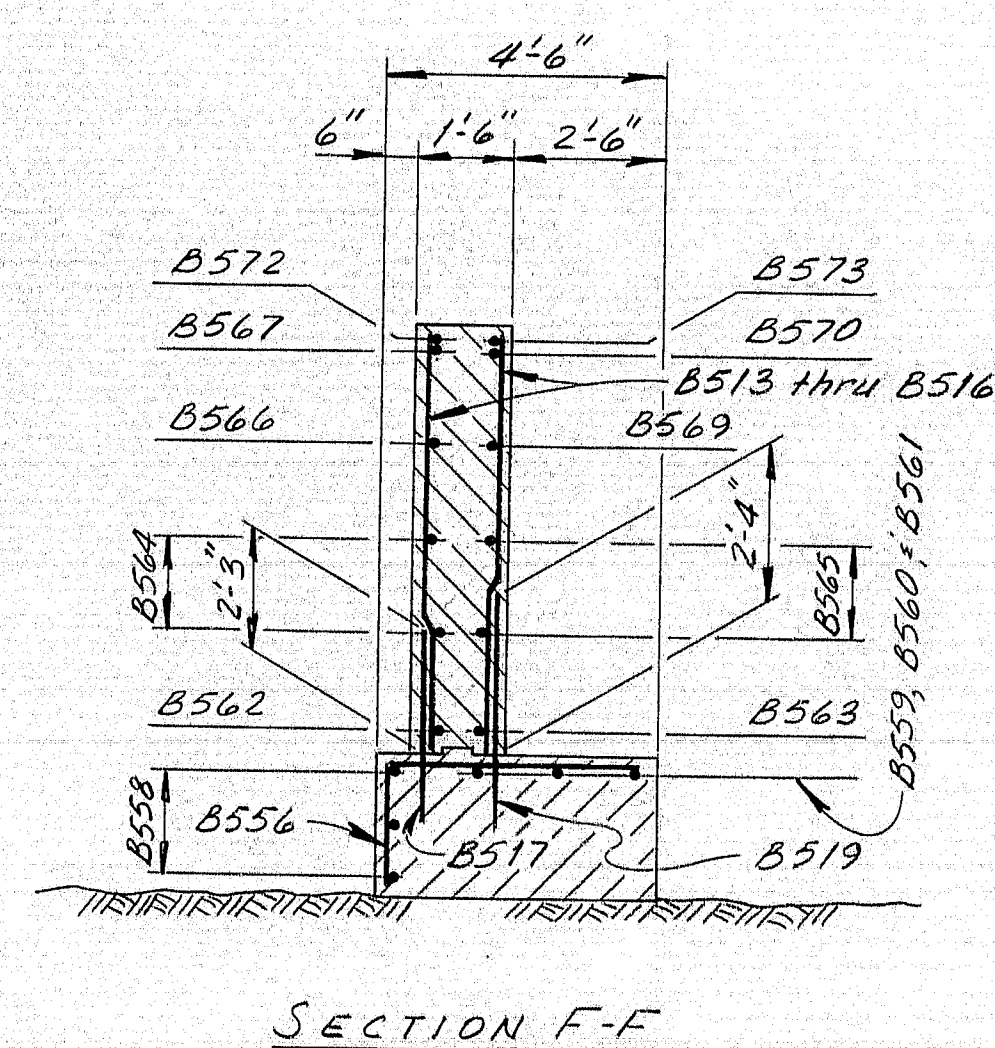
SYMBOLS

Concrete
(Plan or Elev.)
Ledge

R94-308

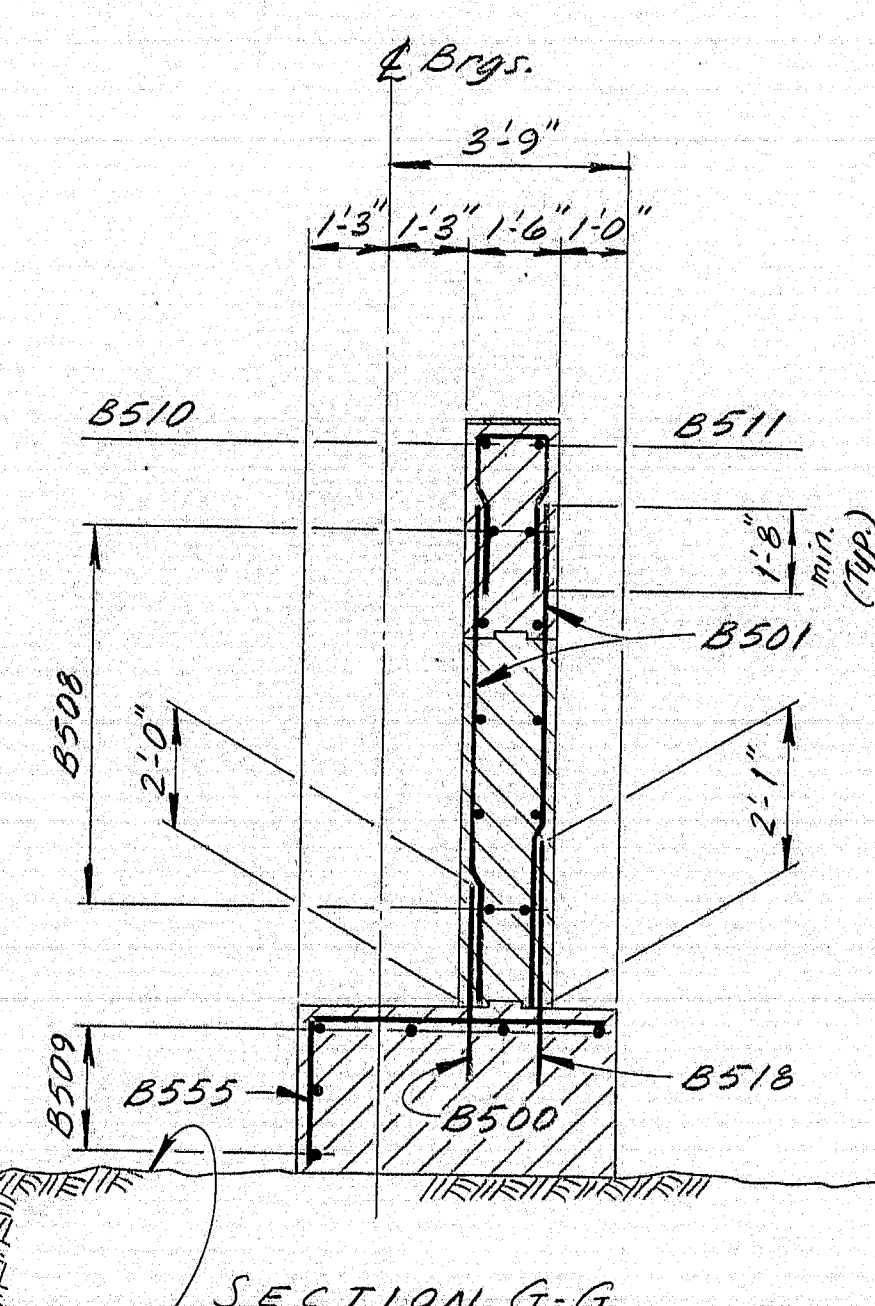
F.H.W.A. REV. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-14-395-B(84)176	8	65

PROJECT DESIGN ENGINEER	BY	DATE
DESIGN - DETAILED	J.E. Gentry	12/20/82
CHECKED	J.E.B.	1/26/83
REVISIONS		
FIELD CHANGES		

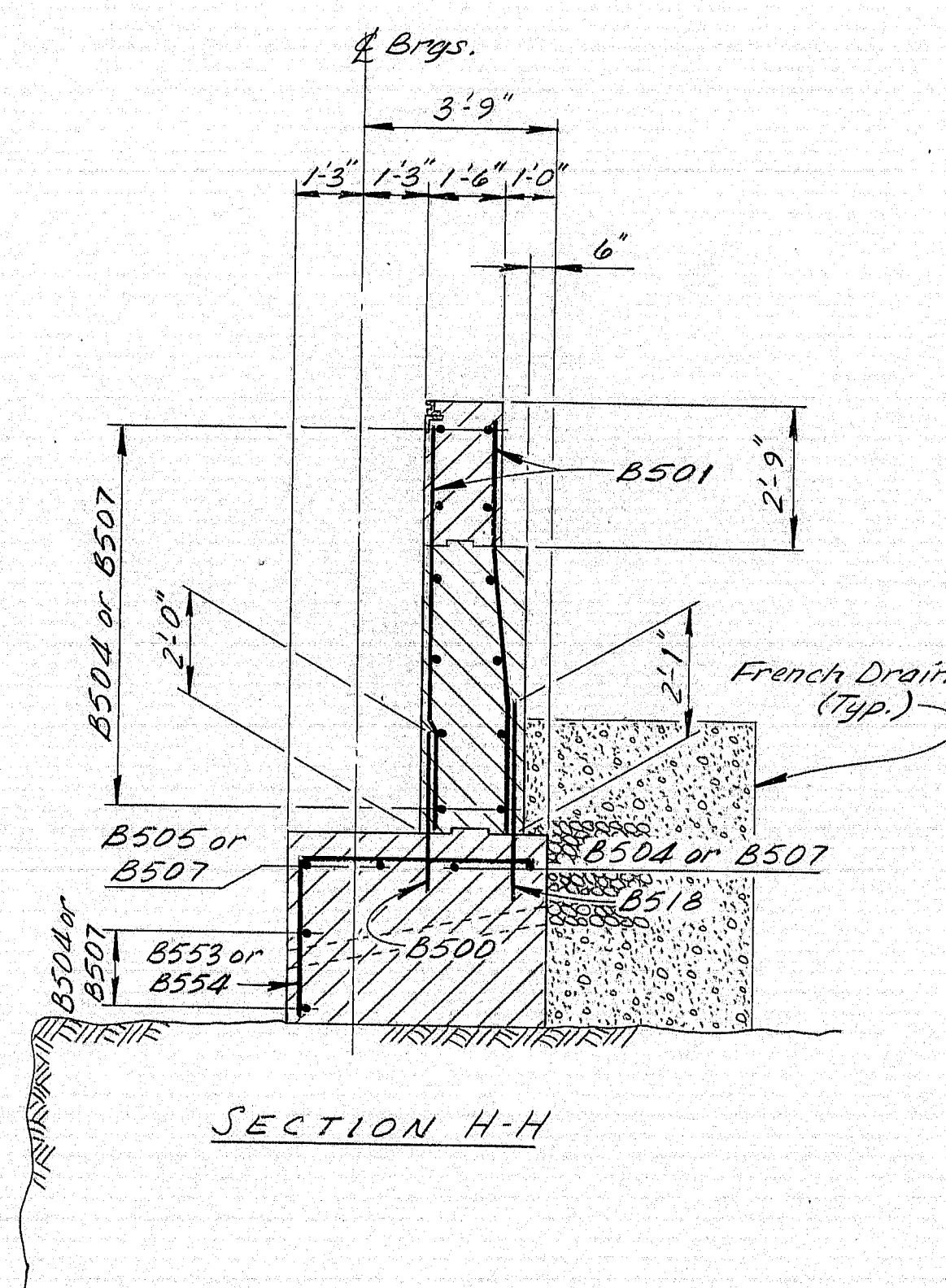


SECTION F-F

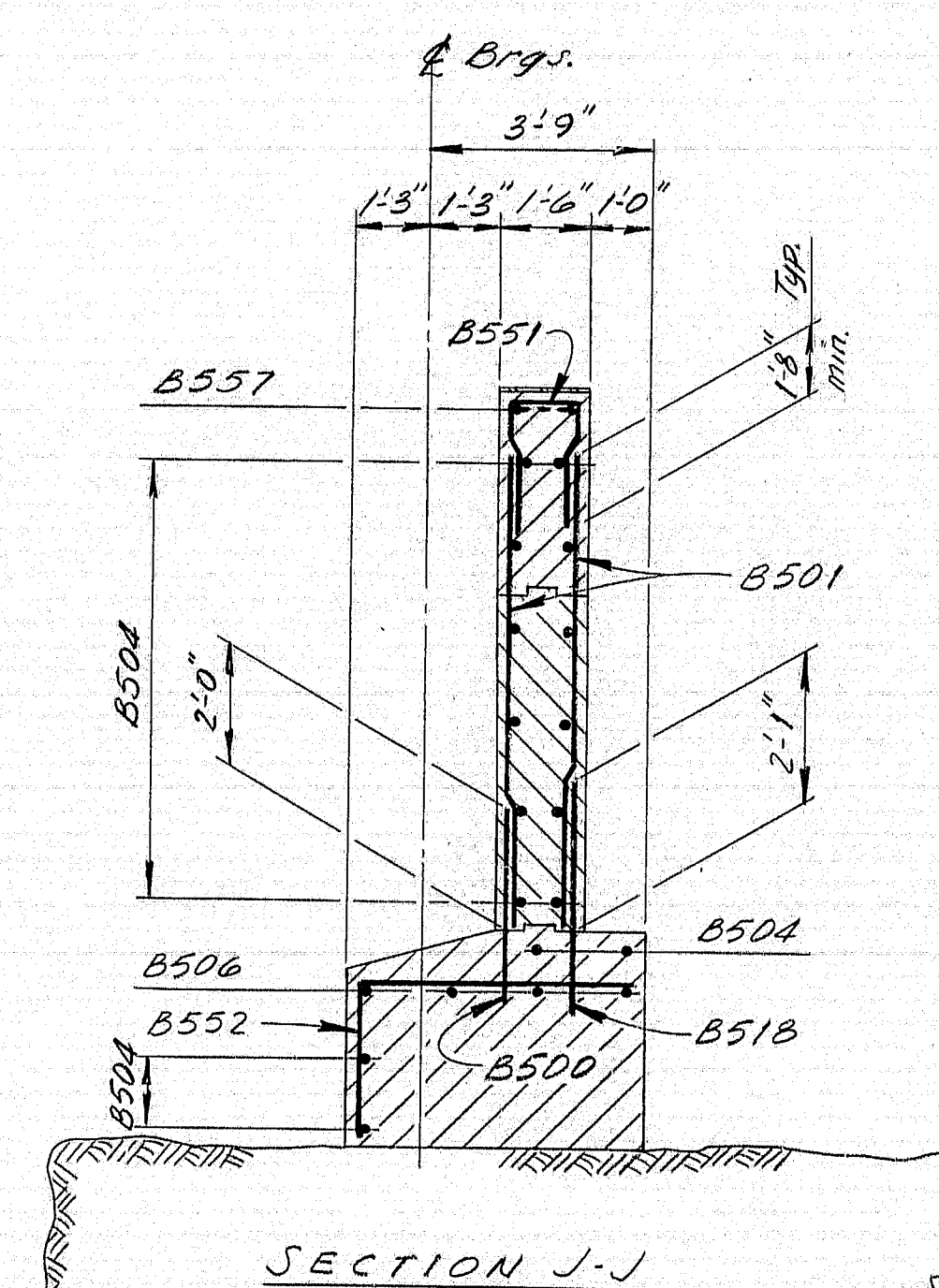
Ledge excavation
by others



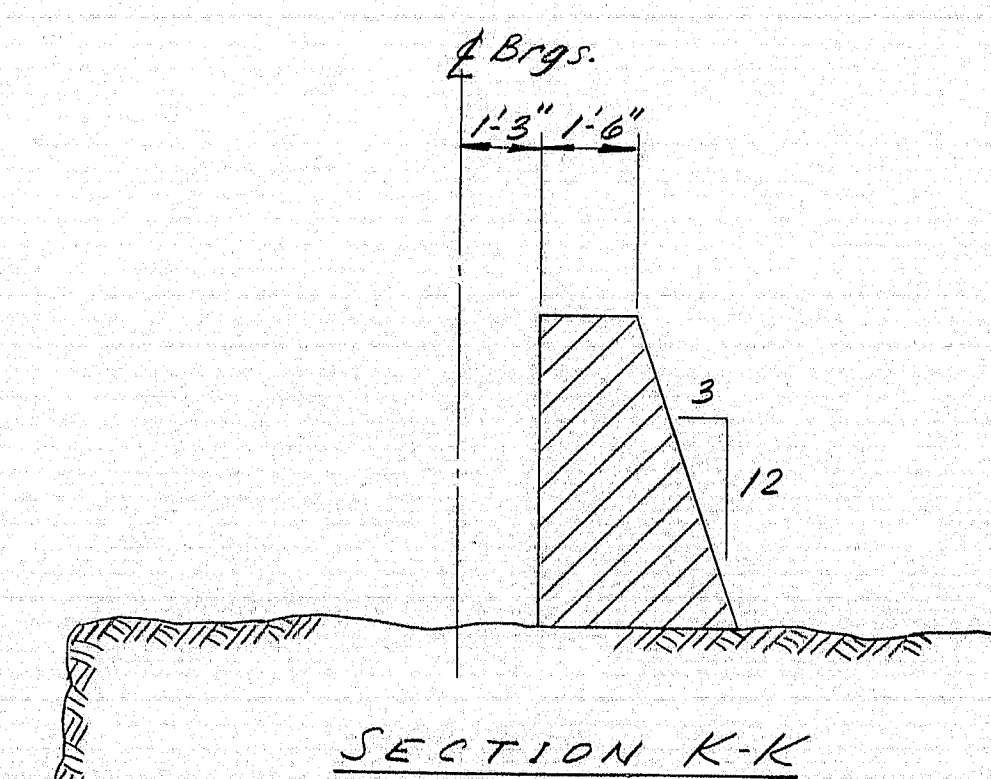
SECTION G-G



SECTION H-H



SECTION J-J



SECTION K-K

"As Built" TAD 12/11/85

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

PARKWAY SOUTH
OVER
INTERSTATE 395

BREWER
PENOBSCOT COUNTY
ABUTMENT No. 2 SECTIONS

SHEET 6 OF 22 AUGUSTA, MAINE April, 1983

R94-309

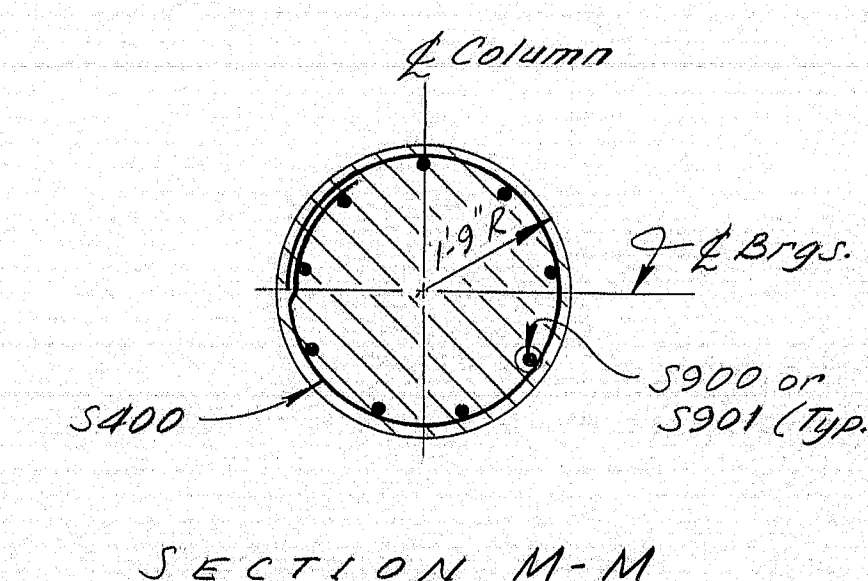
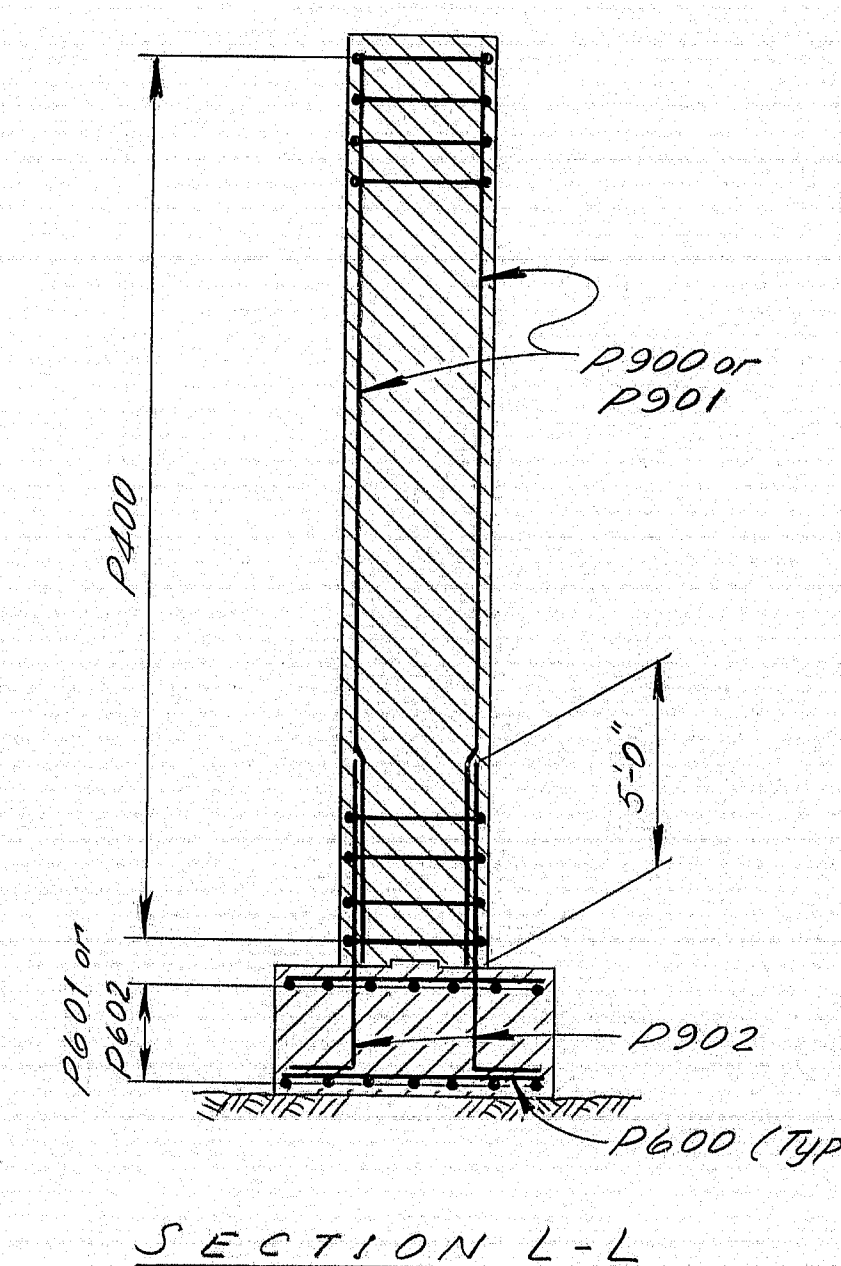
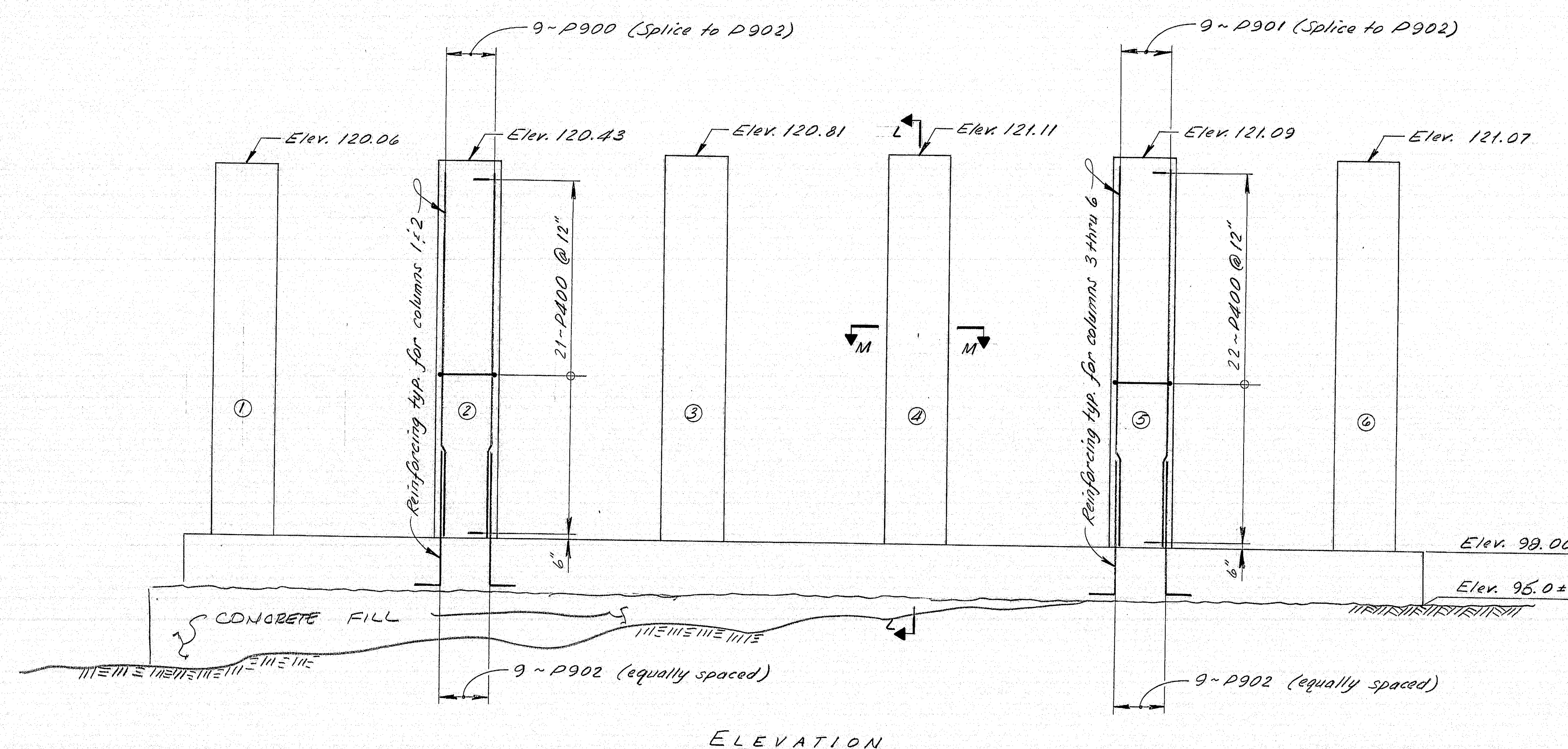
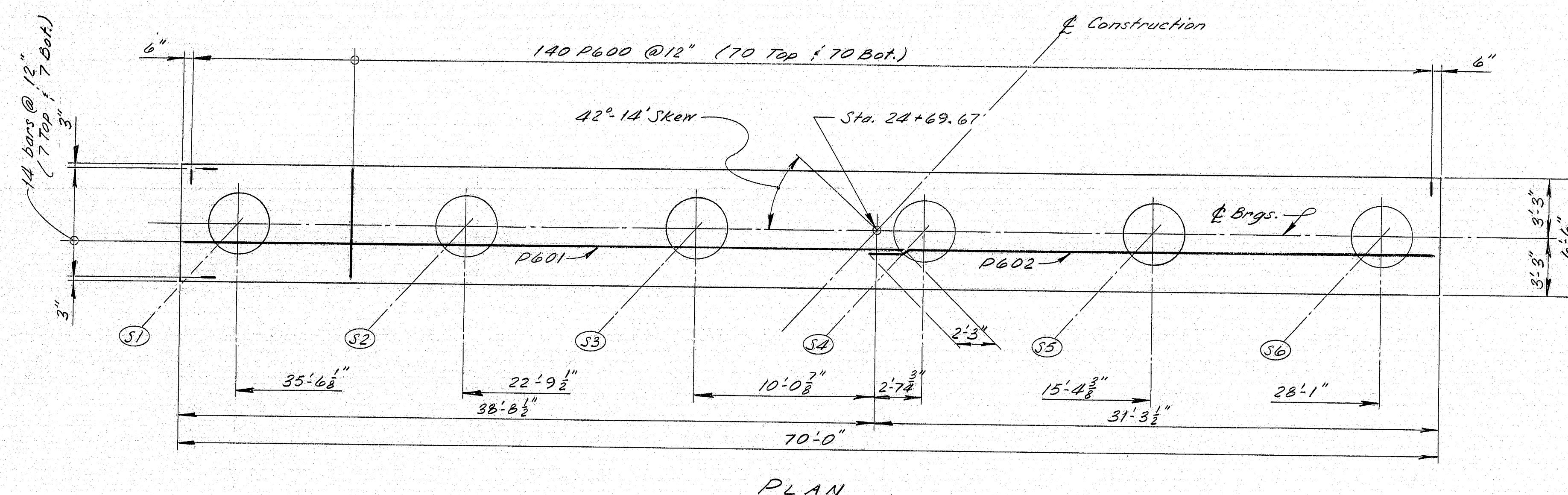
SYMBOLS

	Concrete (Section)
	Ledge

F.H.W.A. NO. 1	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-16-395-B(84)176	9	65

NOTES

- Reinforcing steel shall have 3" minimum cover unless otherwise indicated.
- Design Criteria:
Critical AASHTO Loading - Group II
Wind - 100 mph



Revised As Built TAD 12/11/95

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

PARKWAY SOUTH
OVER
INTERSTATE 395

BREWER
PENOBSCOT COUNTY
PIER

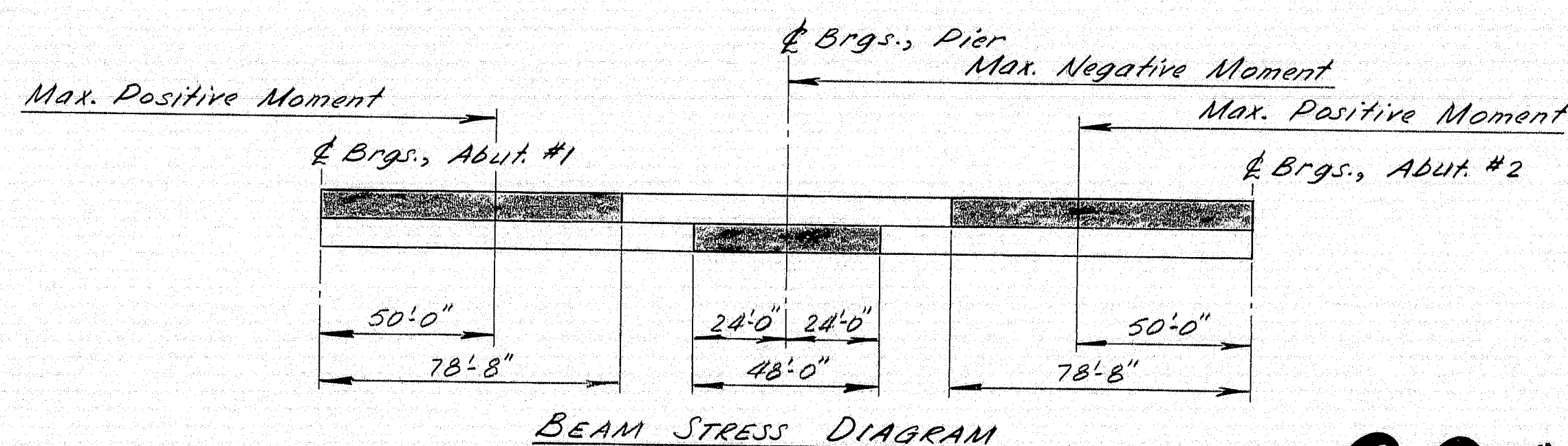
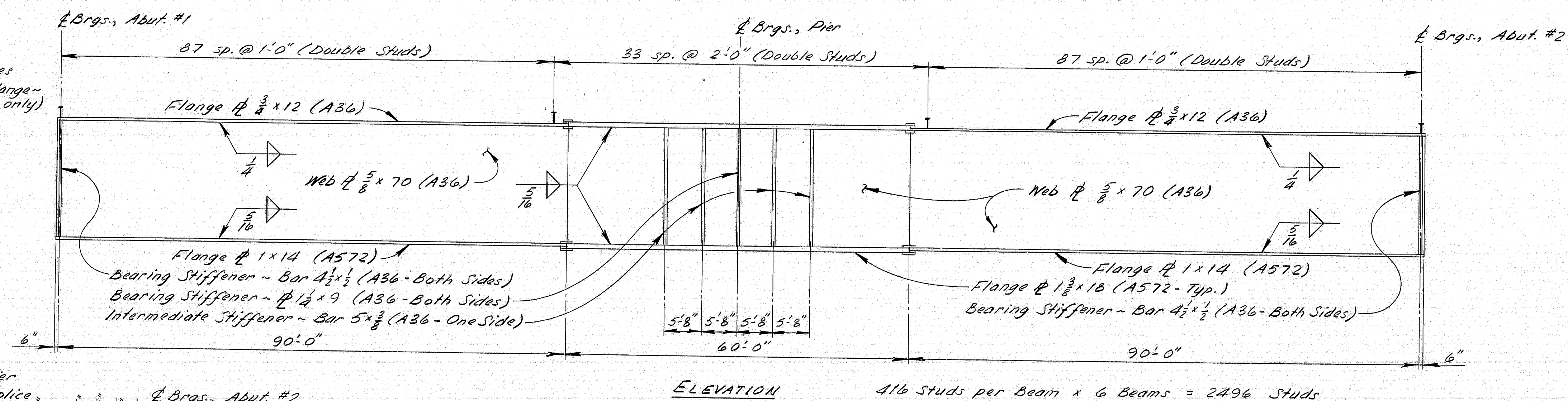
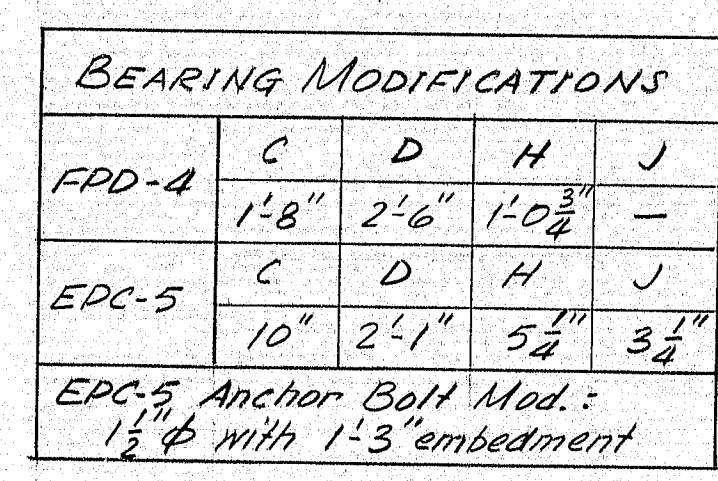
SHEET 7 OF 22 AUGUSTA, MAINE April, 1993

R94-310

PROJECT DESIGN ENGINEER	BY	DATE
DESIGN - DETAILED	U.E. Burrell	12/11/95
CHECKED	J.E. Burrell	1/11/96
REVISIONS		
FIELD CHANGES		

BRUNING 44-132-45710

Note: All diaphragms except as noted shall be Type "L".
(48 Type "L" Diaphragms)



R94-311

AS BUILT TRD 12/11/35

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

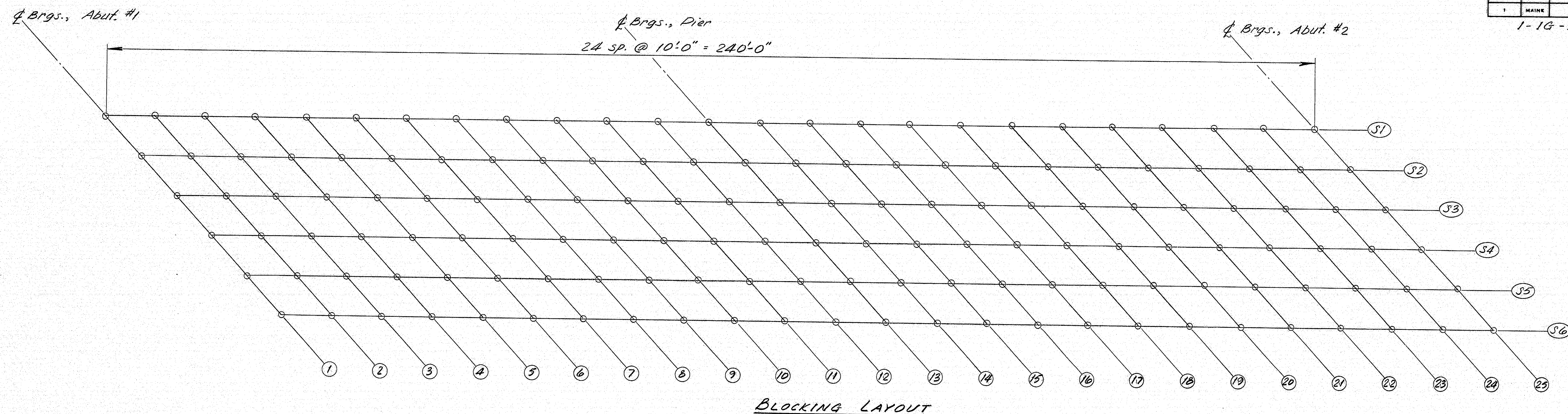
PARKWAY SOUTH
OVER
INTERSTATE 395

BREWER
PENOBSCOT COUNTY

STRUCTURAL STEEL

SHEET 8 OF 22 AUGUSTA, MAINE April

F.H.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-18-395-8(84)176	11	25



	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25
(S1)	124.93	125.19	125.45	125.69	125.93	126.14	126.34	126.54	126.72	126.90	127.08	127.27	127.47	127.69	127.92	128.16	128.41	128.65	128.88	129.10	129.30	129.49	129.67	129.84	130.00
(S2)	125.31	125.57	125.83	126.07	126.30	126.52	126.72	126.91	127.10	127.27	127.45	127.64	127.84	128.06	128.30	128.54	128.78	129.02	129.25	129.47	129.68	129.87	130.05	130.22	130.38
(S3)	125.69	125.95	126.20	126.45	126.68	126.90	127.10	127.29	127.47	127.65	127.83	128.02	128.22	128.44	128.68	128.92	129.16	129.40	129.63	129.85	130.06	130.25	130.42	130.59	130.75
(S4)	125.98	126.24	126.50	126.74	126.97	127.19	127.39	127.58	127.77	127.95	128.13	128.31	128.52	128.74	128.97	129.21	129.46	129.70	129.93	130.15	130.35	130.54	130.72	130.89	131.05
(S5)	125.97	126.23	126.48	126.73	126.96	127.17	127.38	127.57	127.75	127.93	128.11	128.30	128.50	128.72	128.96	129.20	129.44	129.68	129.91	130.13	130.34	130.53	130.70	130.87	131.03
(S6)	125.95	126.21	126.47	126.71	126.94	127.16	127.36	127.55	127.74	127.91	128.09	128.28	128.48	128.71	128.94	129.18	129.42	129.66	129.90	130.11	130.32	130.51	130.69	130.86	131.02

STRUCTURAL STEEL NOTES

1. Camber ordinates, as shown, are computed to compensate for all dead load deflections and for the curvature of the finished grade profile.
2. No transverse butt weld splices will be allowed in the flange plates or web plates within ten feet from the points of maximum negative moment or maximum positive moment.
3. Sections of flange plates or web plates between transverse shop splices or between a transverse shop splice and a field splice shall not be less than 20 feet in length unless otherwise shown on the plans.
4. Butt weld splices in flanges shall be not less than one foot from transverse welds in the web plates.
5. Bearing stiffeners shall be plumb after erection and dead loading of the structure. Intermediate web stiffeners may be either plumb or normal to the top flange.
6. Cross-frame or diaphragm connection plates may be either plumb or normal to the top flange.
7. All field splice connections shall be made with $\frac{1}{2}$ " ASTM A325, Type 1 High Strength Bolts. Holes shall be $\frac{15}{16}$ " ϕ . All splice plates and bars shall be ASTM A36 steel except as noted. Filler plates may be ASTM A36 steel and mill tests for filler plate material will not be required.
8. The Bearing Setting Chart indicates the required final position of the bearings. It is anticipated that the bearings at the Abutments will move $\frac{1}{8}$ " away from the fixed bearings due to placement of the Superstructure concrete. No separate payment will be made for resetting the bearings to the final position if an adjustment is required.
9. Theoretical Blocking shall be $2\frac{1}{2}$ " nominal at Brgs., Abutments and Pier.

MINIMUM ULTIMATE YIELD STRENGTHS

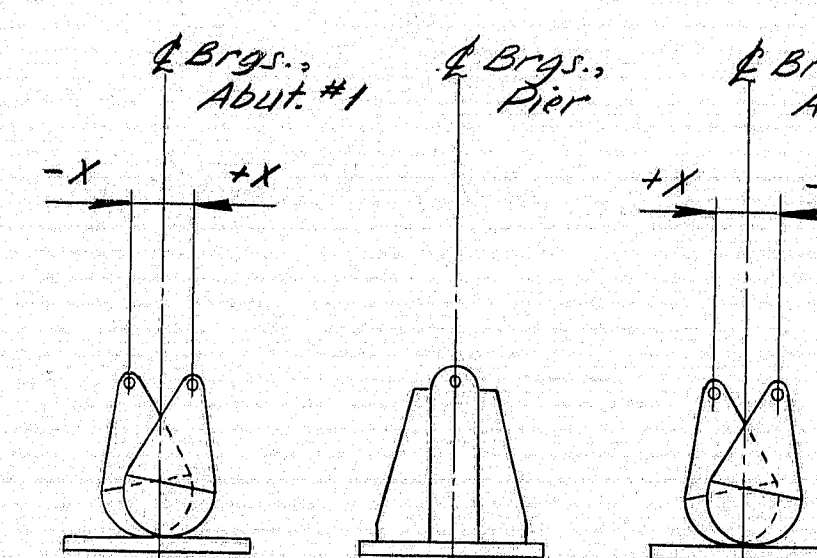
ASTM A572	$F_y = 50,000$ psi
ASTM A36	$F_y = 36,000$ psi

BASIC ALLOWABLE STRESSES

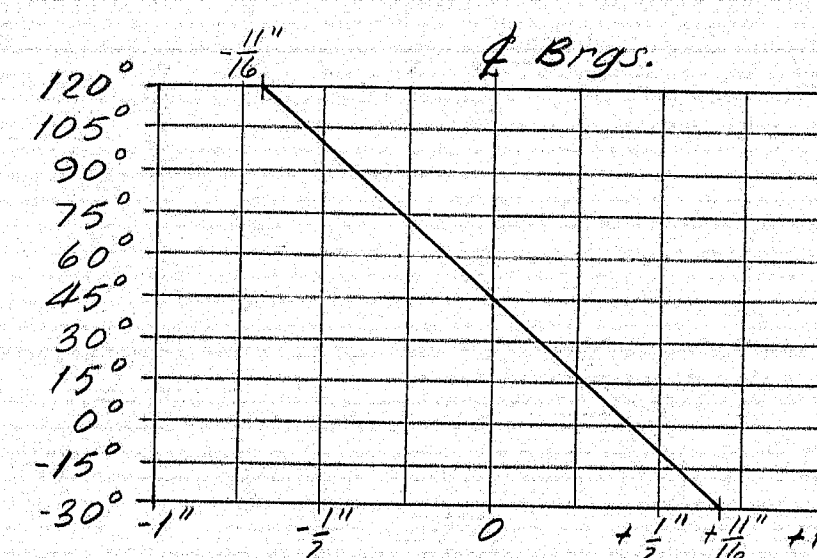
ASTM A325	$f_v = 33,000$ psi
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MATERIALS

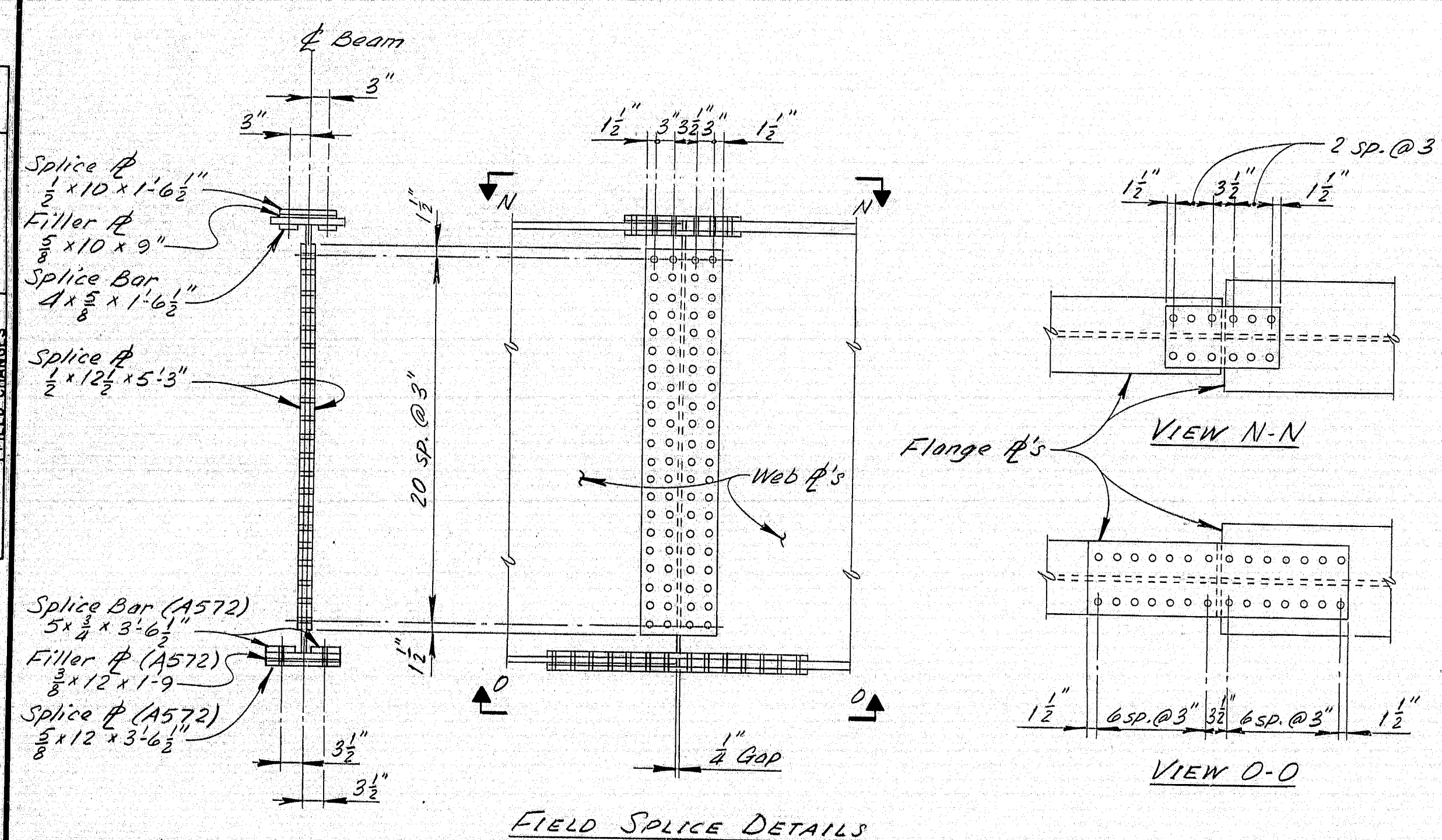
All Material (Except as otherwise noted) — ASTM A36
High Strength Bolts — ASTM A325, Type 1



BEARING SETTING DIAGRAM



BEARING SETTING CHART



FIELD SPLICE DETAILS

PROJECT DESIGN ENGINEER	DATE
LE. Gentry, Jr. Designer	Dec. 82
CHECKED	REVISIONS
TS. B.	1
PLANS	FIELD CHANGES

BRUNING 44-132-45710

As Built TRD 12/1/85

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

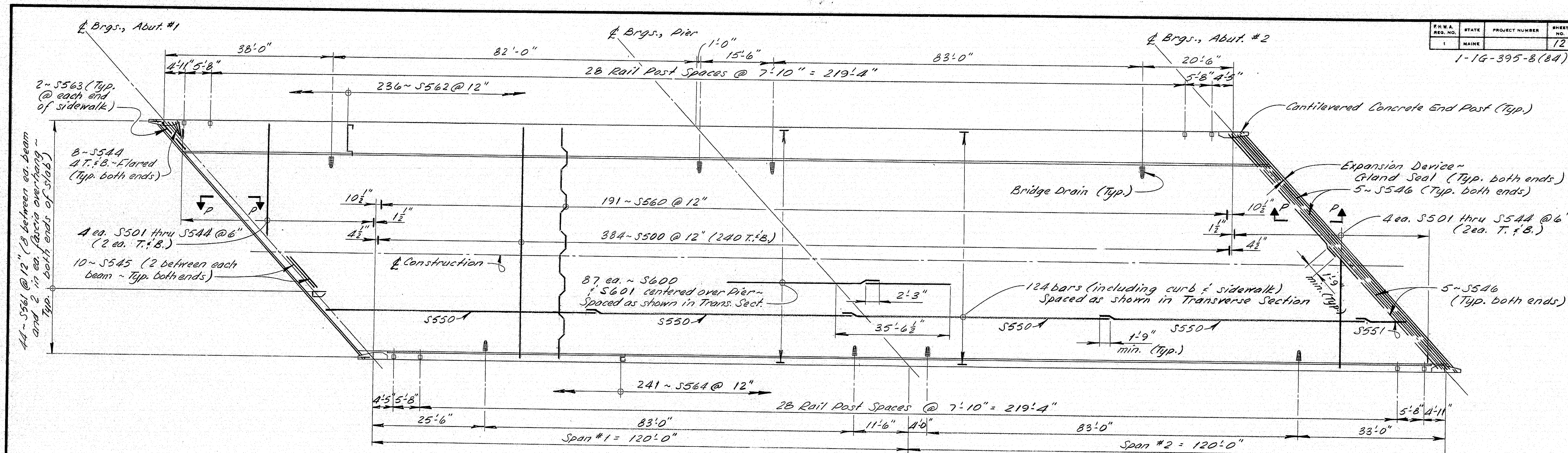
PARKWAY SOUTH
OVER
INTERSTATE 395

BREWER
PENOBSCOT COUNTY
STRUCTURAL STEEL

SHEET 9 OF 22 AUGUSTA, MAINE

R94-312

STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
MAINE	1-16-395-8(84)176	12	65

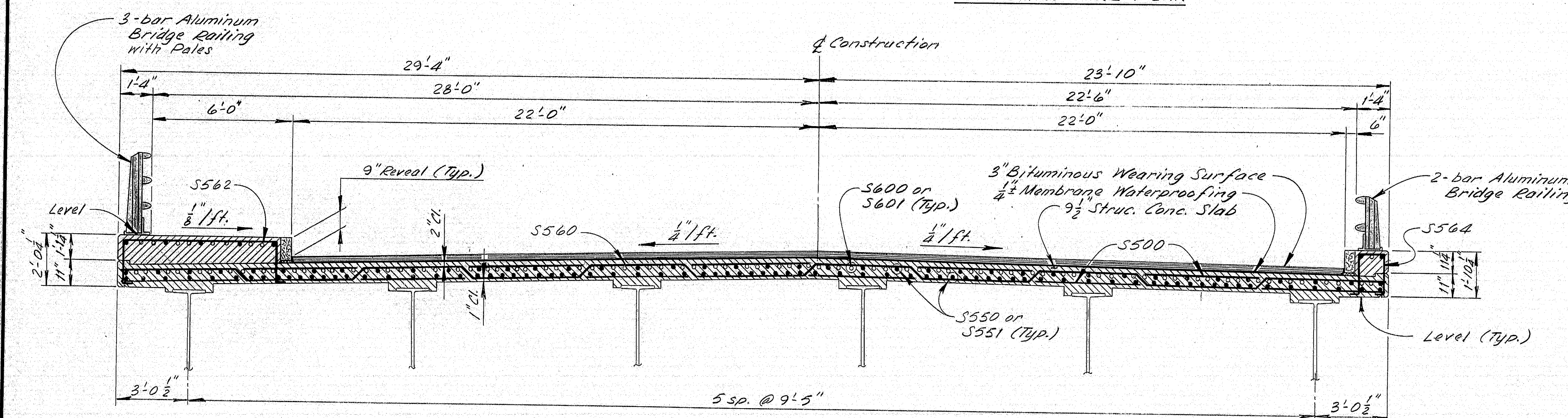


SUPERSTRUCTURE PLAN

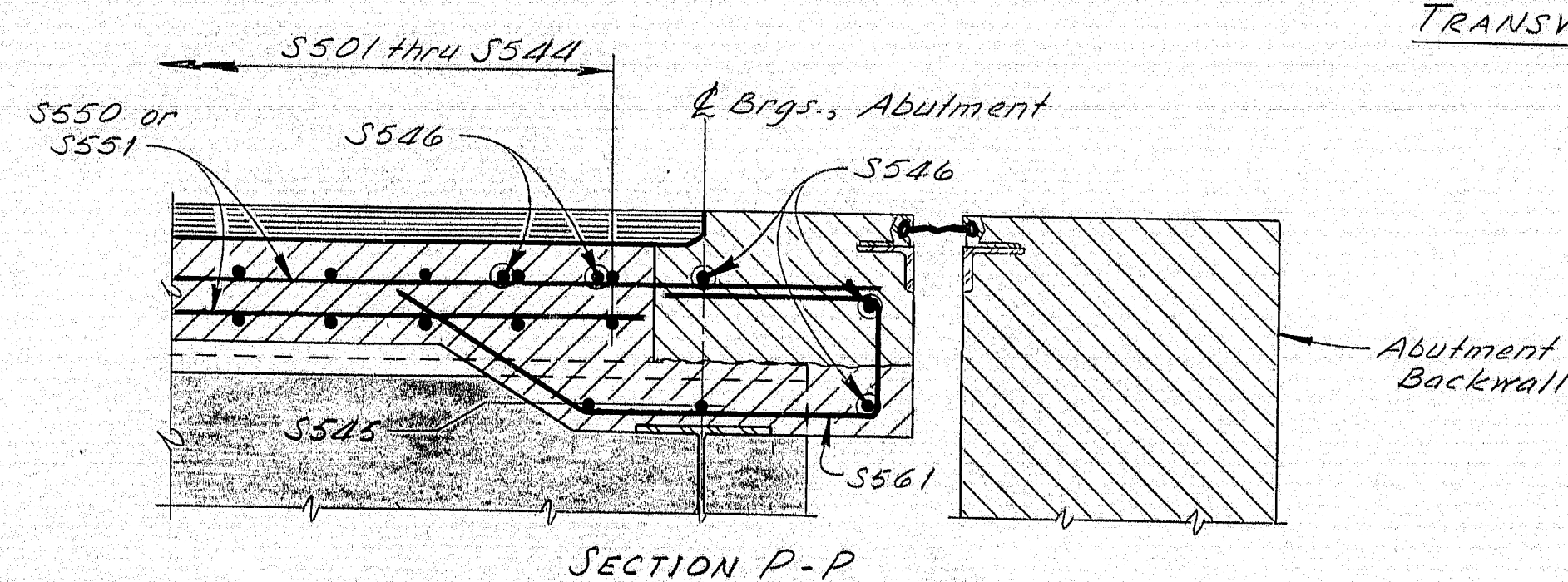
SUPERSTRUCTURE NOTES

- Form a one-inch V-groove on the fascias at the horizontal joint between the curb and slab.
- Reinforcing steel shall have a minimum cover of two inches unless otherwise indicated.
- Adjust reinforcing steel to fit around the drains in a manner approved by the Engineer. Do not cut transverse reinforcing bars.
- The superstructure slab concrete shall be placed in one continuous operation and the concrete shall be kept plastic until the entire superstructure slab has been placed.
- Mortar for bedding and for joints in the granite curb shall contain an approved non-shrink additive.
- Protective Coating for Concrete Surfaces shall be applied to the following areas:

Top of Concrete Curbs
Fascias down to the drip notch
All exposed surfaces of Concrete End Posts
Top of Expansion Device Block-out at ends of slab.



TRANSVERSE SECTION



SECTION P-P

LEGEND

T. & B. = Top & Bottom

SYMBOLS

[Symbol]	Bituminous Pavement
[Symbol]	Concrete (Plan or Elev.)
[Symbol]	Concrete (Section)
[Symbol]	Granite Curb

As Built TMD 12/1/85

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

PARKWAY SOUTH
OVER
INTERSTATE 395

BREWER
PENOBSCOT COUNTY

SUPERSTRUCTURE

SHEET 10 OF 22 AUGUSTA, MAINE April, 1983

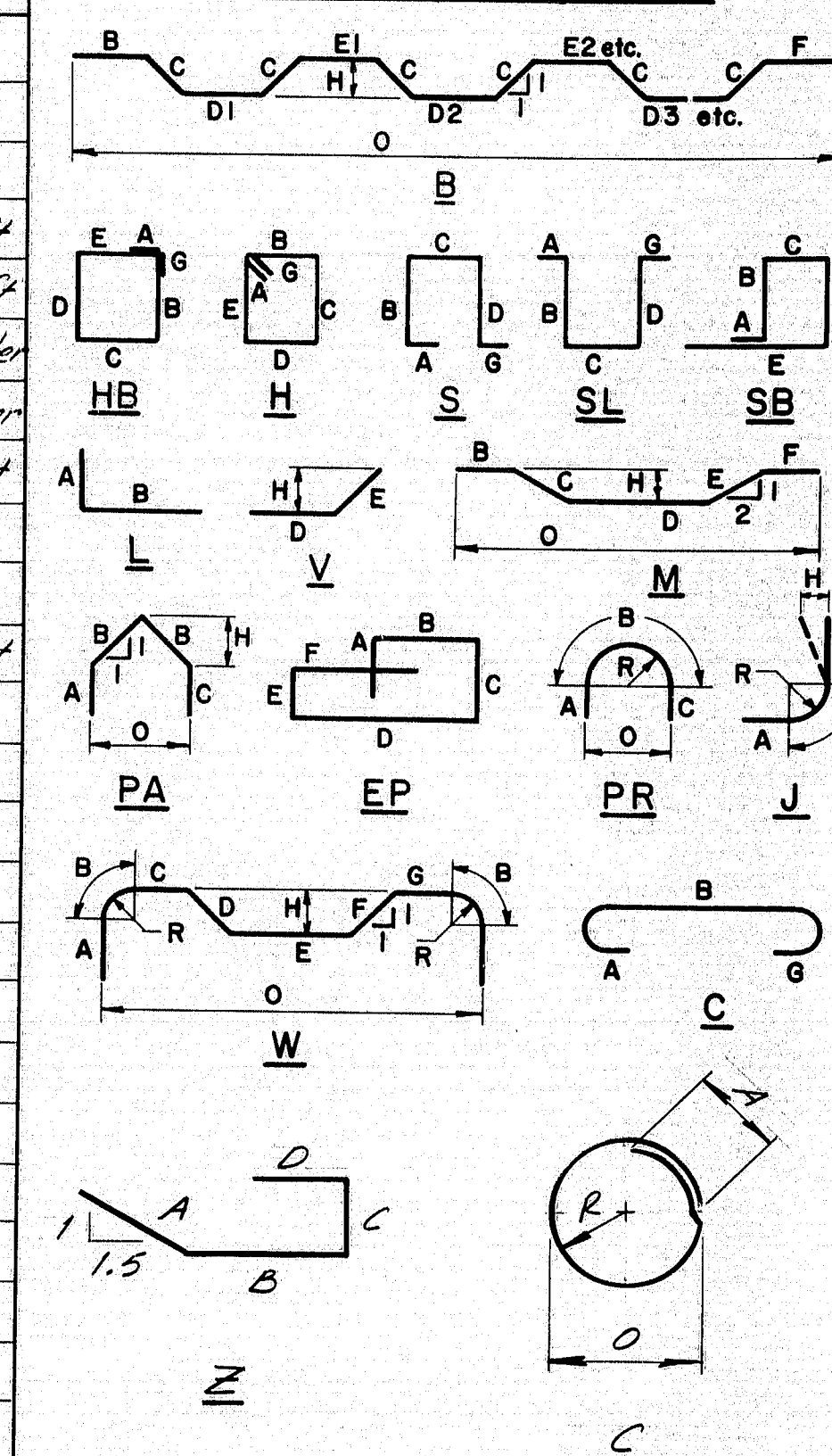
R94-313

REINFORCING STEEL SCHEDULE																										
STRAIGHT BARS				BENT BARS																						
MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	TYPE	A	B	C	D	E	F	G	H	O	R	LOCATION				
ABUTMENT No. 1				SUPERSTRUCTURE				SUPERSTRUCTURE				ABUTMENT No. 1														
A500	51	3'-2"	Dowels	S500	384	52'-10"	Transverse	S550	496	60'-0"	Longitudinal	A550	8	6'-6"	S	0"	2'-8"	1'-2"	2'-8"	—	—	0"	—	Sidewalk		
A501	102	8'-0"	Vertical	S501	8	51'-11"	↑	S551	124	8'-11"	Longitudinal	A551	3	7'-8"	S	0"	3'-3"	1'-2"	3'-3"	—	—	0"	—	Curb		
A502	1	9'-6"	Vertical	S502	8	50'-10"	↑					A552	3	9'-6"	L	4'-10"	4'-8"	—	—	—	—	—	—	Breastwall-Left		
A503	2	1'-9"	Curb	S503	8	49'-9"	↑	S600	87	53'-4"	Longitudinal	A553	15	10'-1"	L	5'-5"	4'-8"	—	—	—	—	—	—	Breastwall-Left		
A504	2	17'-2"	Horiz.-Left Side	S504	8	48'-8"	↑	S601	87	20'-0"	Longitudinal	A554	9	10'-9"	L	6'-1"	4'-8"	—	—	—	—	—	—	Breastwall-Cent		
A505	1	12'-4"	Sidewalk	S505	8	47'-7"	↑					A555	8	11'-1"	L	6'-5"	4'-8"	—	—	—	—	—	—	Breastwall-Cent		
A506	1	11'-0"	Sidewalk	S506	8	46'-5"	↑					A556	14	13'-8"	L	9'-6"	4'-2"	—	—	—	—	—	—	Breastwall-Right		
A507	15	26'-8"	Horiz.-Left Side	S507	8	45'-4"	↑					A557	6	20'-7"	V	—	—	2'-8"	17'-11"	—	—	15'-6"	—	Horiz.-Wing		
A508	2	21'-6"	Horiz.-Left Side	S508	8	44'-3"	↑					A558	6	23'-6"	V	—	—	2'-0"	21'-6"	—	—	18'-7 1/2"	—	Horiz.-Wing		
A509	4	6'-5"	Horiz.-Left Side	S509	8	43'-2"	↓	PIER				A559	1	13'-0"	L	8'-10"	4'-2"	—	—	—	—	—	Breastwall-Right			
A510	20	25'-2"	Horiz.-Center	S510	8	42'-1"	Transverse	P600	140	6'-0"	Footing		1		V											
A511	24	22'-3"	Horiz.-Right Side	S511	8	40'-11"	↑	P601	14	40'-0"	Footing	A561	1	20'-1"	V	—	—	2'-8"	17'-5"	—	—	15'-1"	—	—	Horiz.-Wing	
A512	4	21'-5"	Horiz.-Right Side	S512	8	39'-10"	↑	P602	14	31'-9"	Footing	A562	1	16'-11"	V	—	—	2'-8"	14'-3"	—	—	12'-4"	—	—	↑	
A513	4	2'-6"	Vert.-Wing	S513	8	38'-9"	↑					A563	1	13'-9"	V	—	—	2'-8"	11'-1"	—	—	9'-7"	—	—		
A514	4	3'-11"	↑	S514	8	37'-8"	↑	P900	18	20'-10"	Columns	A564	1	10'-7"	V	—	—	2'-8"	7'-11"	—	—	6'-10"	—	—		
A515	4	5'-4"	↑	S515	8	36'-7"	↑	P901	36	21'-7"	Columns	A565	1	7'-5"	V	—	—	2'-8"	4'-9"	—	—	4'-1 1/2"	—	—		
A516	4	6'-9"	↓	S516	8	35'-5"	↑					A566	1	4'-4"	V	—	—	2'-8"	1'-8"	—	—	1'-5 1/2"	—	—		
A517	4	8'-2"	Vert.-Wing	S517	8	34'-4"	↑					A567	1	21'-9"	V	—	—	3'-6"	18'-3"	—	—	15'-9 3/4"	—	—		
A518	35	3'-4"	Dowels	S518	8	33'-3"	↑																			

FHWA REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE		13	66

1-1G-395-8(84)17

TYPE-BENDING DIAGRAMS



All dimensions are out to out of reinf. bar

Bending details and hooks shall conform to the recommendations of ACI Standard 315-65.

Reinforcing Bar: ASTM A615 Grade 60

GENERAL NOTES

1. First digit(s) following the letter of the Mark indicates size of reinf. bar.
 Mark (A 502) bar size - #5
 Mark (P 1001) bar size - #10
 Mark (S 603) bar size - #6
2. Letter of Marks A, P, & S locates bars of Abutments, Piers, and Superstructure parts respectively.

7/5 Bu14 TAD 12/11/95

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

PARKWAY SOUTH
OVER
INTERSTATE 395

BREWER
PENOBSCOT COUNTY

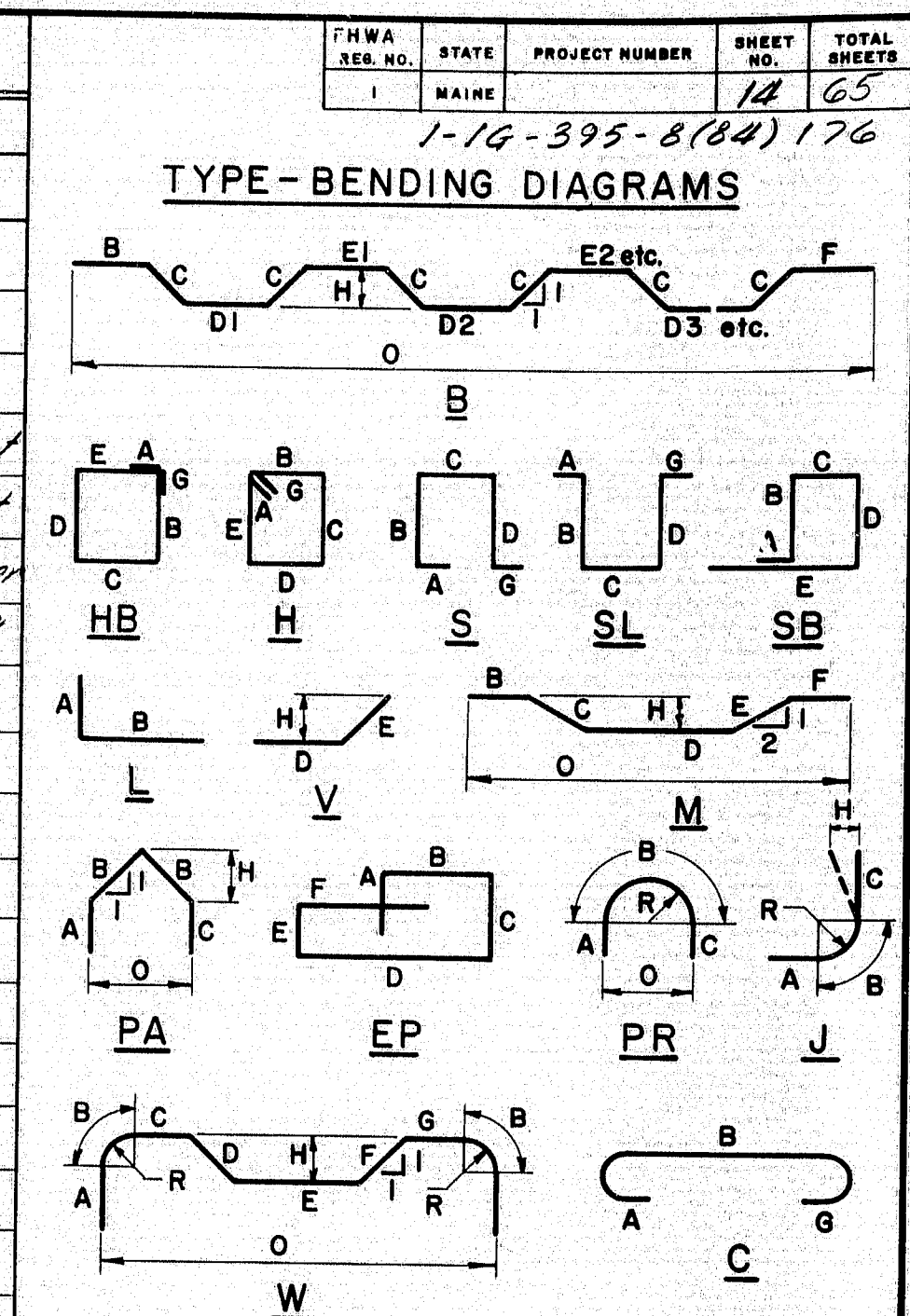
REINFORCING STEEL SCHEDULE

SHEET 11 OF 22 AUGUSTA, MAINE April, 1983

R94-314

REINFORCING STEEL SCHEDULE

REINFORCING STEEL SCHEDULE																													
STRAIGHT BARS															BENT BARS														
MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	TYPE	A	B	C	D	E	F	G	H	O	R	LOCATION			
ABUTMENT No. 2				END POSTS								ABUTMENT No. 2																	
B500	60	3'-2"	Dowels	EP401	16	1'-10"	Dowels					B550	8	6'-0"	S	0"	2'-5"	1'-2"	2'-5"	—	—	0"	—	—	—	—	Sidewalk		
B501	102	8'-0"	Vertical	EP405	8	1'-5"	Vertical					B551	3	6'-0"	S	0"	2'-5"	1'-2"	2'-5"	—	—	0"	—	—	—	—	Curb		
B502	1	9'-6"	Vertical	EP407	8	2'-5"	Vertical					B552	3	7'-4"	L	2'-8"	4'-8"	—	—	—	—	—	—	—	—	—	Breastwall-Right		
B503	1	1'-9"	Curb									B553	23	8'-0"	L	3'-4"	4'-8"	—	—	—	—	—	—	—	—	—	Breastwall-Right		
B504	16	26'-8"	Horiz.- Right Side	EP708	18	4'-0"	Horizontal					B554	9	7'-9"	L	3'-1"	4'-8"	—	—	—	—	—	—	—	—	—	Breastwall-Cent		
B505	2	21'-3"	Horiz.- Right Side									B555	15	7'-0"	L	2'-4"	4'-8"	—	—	—	—	—	—	—	—	—	Breastwall-Left		
B506	4	6'-5"	Horiz.- Right Side									B556	11	6'-1"	L	1'-11"	4'-2"	—	—	—	—	—	—	—	—	—	Wing Footing		
B507	18	25'-2"	Horiz.- Center									B557	1	9'-10"	S	0"	3'-8"	1'-2"	5'-0"	—	—	—	—	—	—	—	Horiz.- Curb		
B508	10	20'-8"	Horiz.- Left Side									B558	3	19'-4"	V	—	—	—	4'-1"	15'-3"	—	—	—	—	—	—	Wing Footing		
B509	6	21'-7"	Horiz.- Left Side									B559	1	17'-8"	V	—	—	—	3'-8"	14'-0"	—	—	—	—	—	—			
B510	1	8'-2"	Horiz.- Sidewalk									B560	1	16'-2"	V	—	—	—	3'-4"	12'-10"	—	—	—	—	—	—			
B511	1	9'-8"	Horiz.- Sidewalk									B561	1	14'-7"	V	—	—	—	3'-0"	11'-7"	—	—	—	—	—	—	Wing Footing		
B512	2	13'-2"	Horiz.- Left Side									B562	1	14'-10"	V	—	—	—	2'-9"	12'-1"	—	—	—	—	—	—	Horiz.- Wing		
B513	4	4'-5"	Vert.-Wing									B563	1	13'-4"	V	—	—	—	2'-0"	11'-4"	—	—	—	—	—	—			
B514	4	5'-9"										B564	2	17'-9"	V	—	—	—	5'-8"	12'-1"	—	—	—	—	—	—			
B515	4	7'-2"										B565	2	16'-5"	V	—	—	—	5'-0"	11'-5"	—	—	—	—	—	—			
B516	4	8'-7"	Vert.-Wing									B566	1	16'-7"	V	—	—	—	5'-8"	10'-11"	—	—	—	—	—	—			
B517	8	3'-5"	Dowels									B567	1	13'-4"	V	—	—	—	5'-8"	7'-8"	—	—	—	—	—	—			
B518	51	3'-4"	Dowels									B568	1	10'-0"	V	—	—	—	5'-8"	4'-4"	—	—	—	—	—	—			
B519	8	3'-7"	Dowels									B569	1	15'-3"	V	—	—	—	5'-0"	10'-3"	—	—	—	—	—	—			
												B570	1	12'-0"	V	—	—	—	5'-0"	7'-0"	—	—	—	—	—	—			
												B571	1	8'-9"	V	—	—	—	5'-0"	3'-9"	—	—	—	—	—	—			
												B572	1	18'-9"	V	—	—	—	5'-8"	13'-1"	—	—	—	—	—	—			
												B573	1	17'-5"	V	—	—	—	5'-0"	12'-5"	—	—	—	—	—	—	Horiz.-Wing		
END POSTS															END POSTS														
												EP402	18	4'-9"	S	0"	2'-1"	7"	2'-1"	—	—	0"	—	—	—	—	Horizontal		
												EP403	18	4'-9"	H	4"	1'-0"	1'-0"	1'-0"	1'-0"	—	4"	—	—	—	—	Horizontal		
												EP404	8	3'-1"	S	0"	1'-3"	7"	1'-3"	—	—	0"	—	—	—	—	Vertical		
												EP406	8	3'-11"	S	0"	1'-8"	7"	1'-8"	—	—	0"	—	—	—	—			
												EP408	6	4'-3"	S	0"	1'-10"	7"	1'-10"	—	—	0"	—	—	—	—			
												EP409	4	4'-2"	S	0"	1'-10"	6"	1'-10"	—	—	0"	—	—	—	—			
												EP410	2	4'-6"	S	0"	1'-10"	10"	1'-10"	—	—	0"	—	—	—	—			
												EP411	6	6'-9"	S	0"	3'-1"	7"	3'-1"	—	—	0"	—	—	—	—			
												EP412	4	6'-8"	S	0"	3'-1"	6"	3'-1"	—	—	0"	—	—	—	—			
												EP413	2	7'-0"	S	0"	3'-1"	10"	3'-1"	—	—	0"	—	—	—	—	Vertical		
												EP701	18	5'-3"	V	—	—	—	3'-0"	2'-3"	—	—	4"	—	—	—	Horizontal		
												EP702	12	4'-11"	S	0"	1'-11"	7"	1'-11"	—	—	6"	—	—	—	—	Vertical		
												EP703	8	4'-10"	S	0"	1'-11"	6"	1'-11"	—	—	6"	—	—	—	—	Vertical		
												EP704	4	6'-5"	H	5"	1'-11"	10"	1'-11"	10"	—	5"	—	—	—	—	Vertical		
MARK	NO.	LENGTH	TYPE	A	B	C	D	E	F	G	H	O	R	LOCATION															



All dimensions are out to out of reinf. bar.
Bending details and hooks shall conform to the recommendations of ACI Standard 315-65.
Reinforcing Bar: ASTM A615 Grade 60

GENERAL NOTES

- First digit(s) following the letter of the Mark indicates size of reinf. bar.
Mark (A 502) bar size - #5
Mark (P 1001) bar size - #10
Mark (S 603) bar size - #6
- Letter of Marks A, P & S locates bars of Abutments, Piers, and Superstructure parts respectively.

Revised "As Built" TRD 12/11/65

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

PARKWAY SOUTH
OVER
INTERSTATE 395

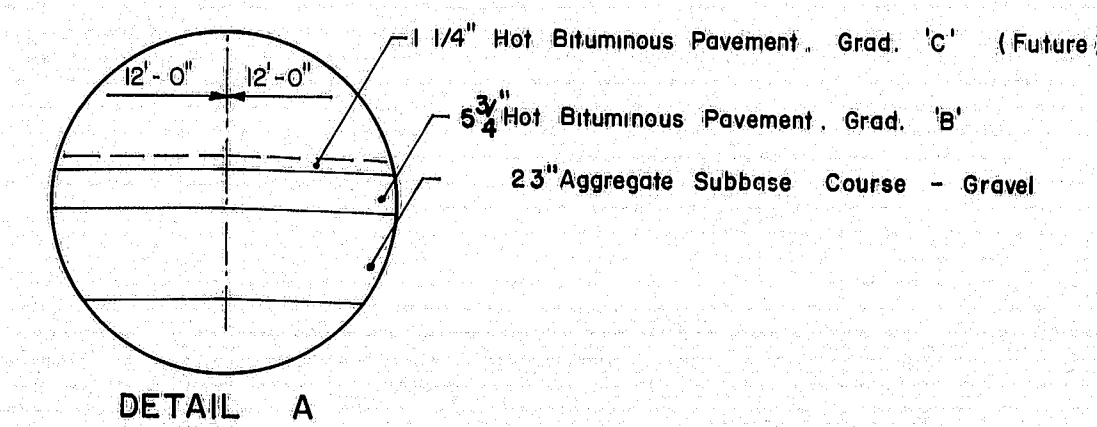
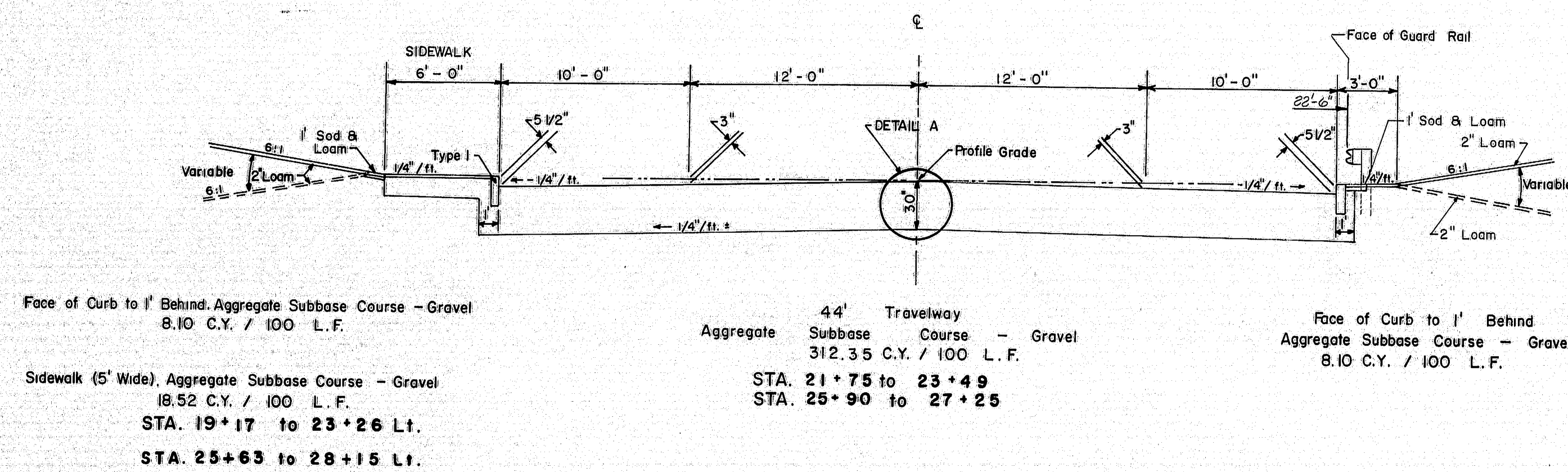
BREWER
DENOBSCOT COUNTY
REINFORCING STEEL SCHEDULE

SHEET 12 OF 22 AUGUSTA, MAINE April, 1983

R94-315

A HOT BITUMINOUS PAVEMENT

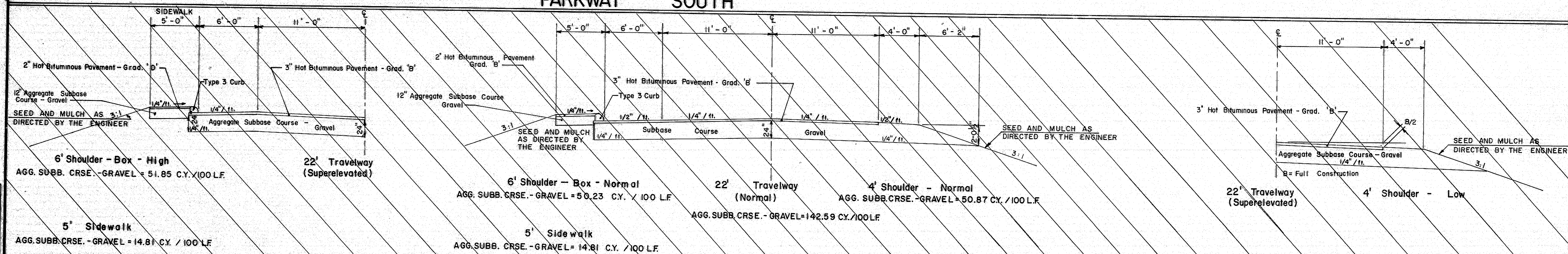
F.H.A. PLAN NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	10 285 8(4)	15	65



NOTES:

- 1) The pavement and base depths as shown on the plans are intended to be nominal.
- 2) Crowns for both normal and super-elevated sections for all courses of subbase, base and pavement shall be straight.
- 3) When super-elevation exceeds 3/4" / ft. low side shoulder shall have same slope as pavement.
- 4) For limits of Stage Construction see profile and cross sections.

PARKWAY SOUTH

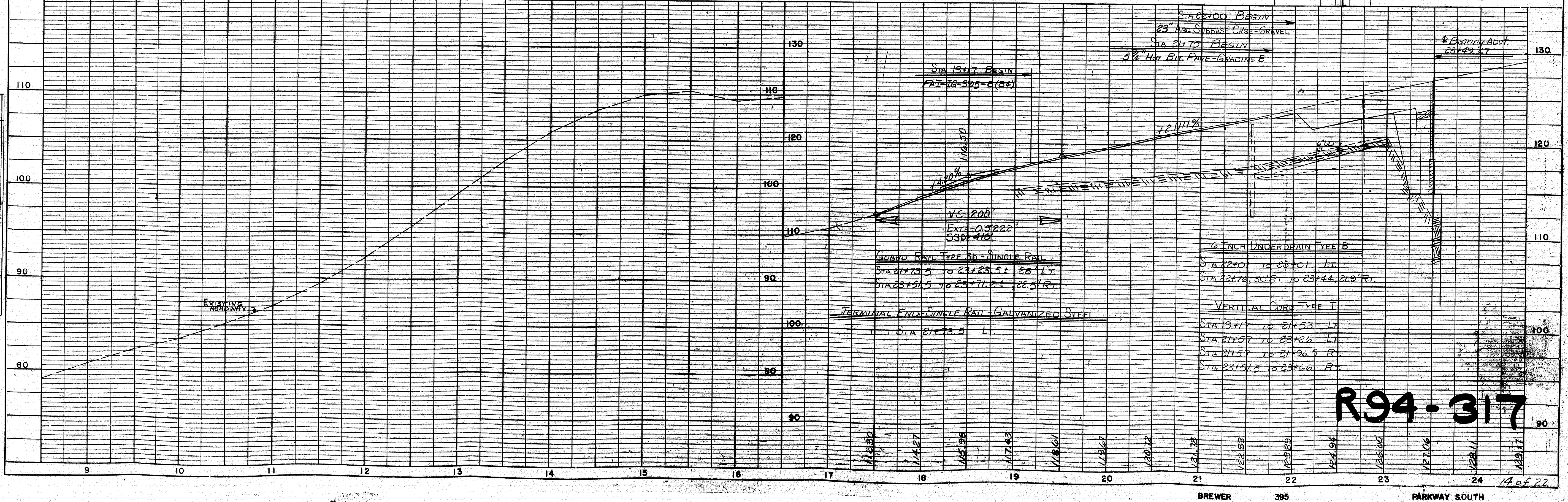
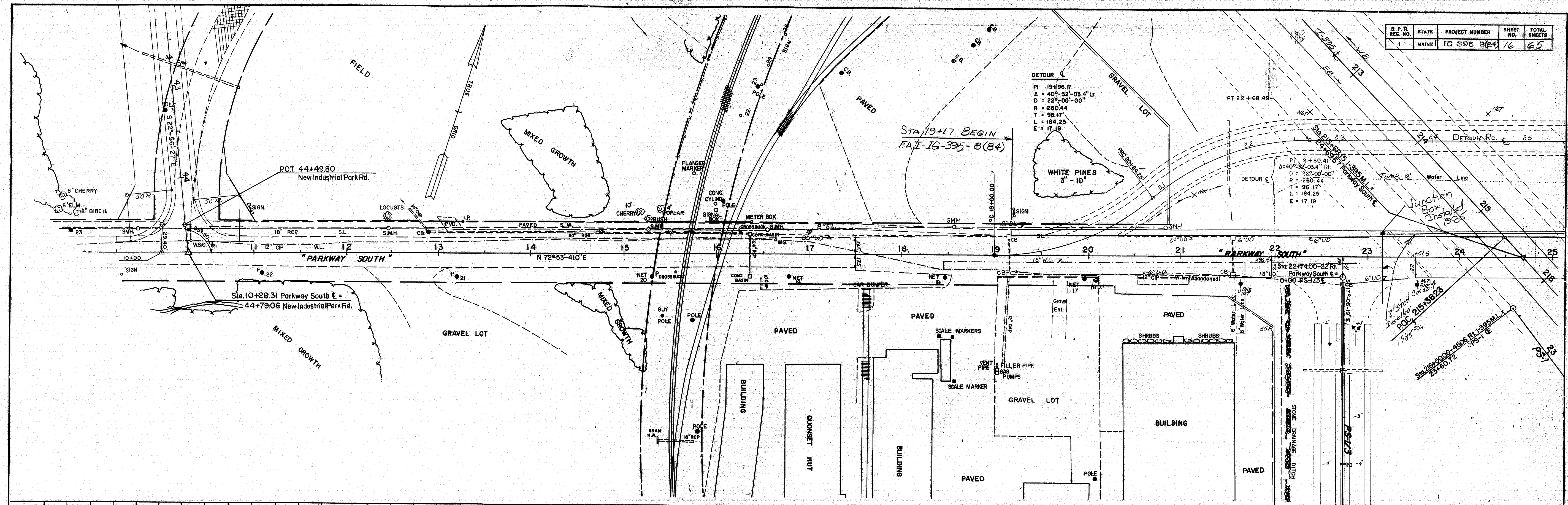


PARKWAY SOUTH DETOUR
(NOT PART OF THIS PROJECT)

"As Built" TAD 12/1/85
 STATE OF MAINE
 DEPARTMENT OF TRANSPORTATION

TYPICAL SECTION
 PARKWAY SOUTH

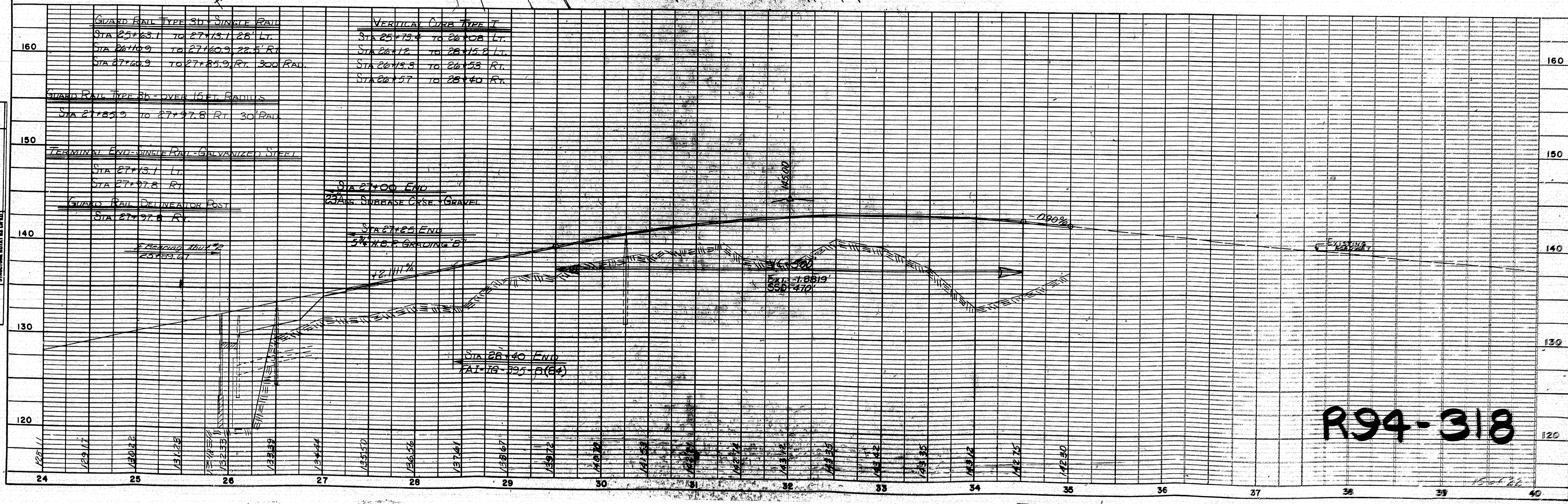
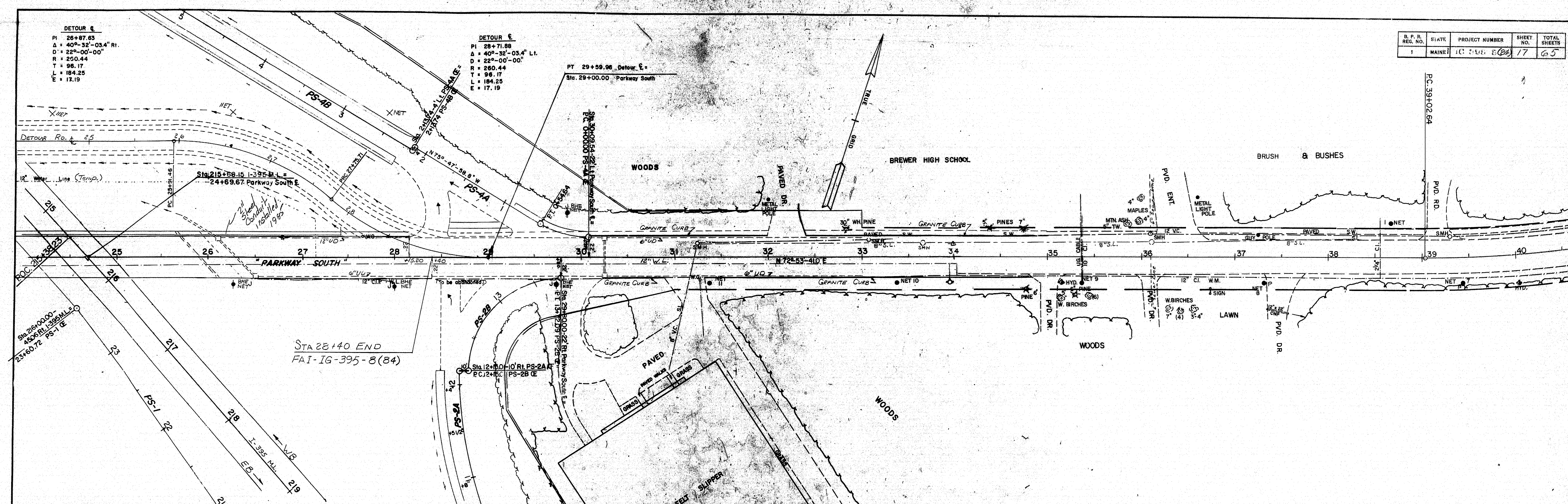
R94-316



S. P. E.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	10 S95 B(84)	76	65

PROFILE	DATE
SHEETED	
NOTED	
NO.	

R94-317



R94-318

FINAL SURVEY	SURVEYED	BY	DATE
	PLOTTED		
	TEMPLATE		
	AREAS		
NOTE BOOK			

ORIGINAL SURVEY		BY	DATE
		Robison F. Craw	9-30-77
		Emery F. Kirk	12-11-81
NOTE BOOK			
PLOTTED			
TEMPLATE			
AREAS			
AREAS CHECKED			
NO.	395/26		

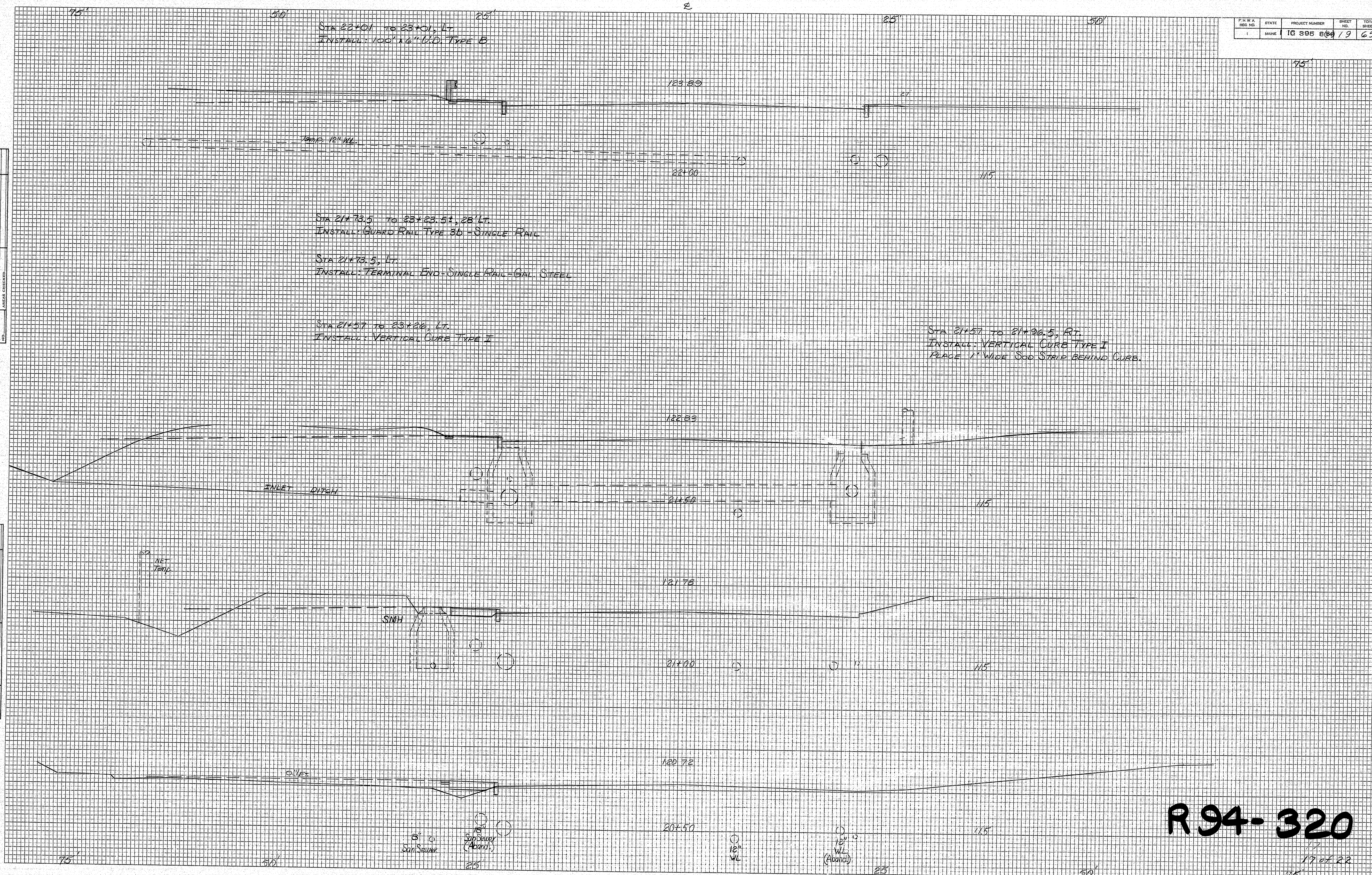
R94-319

PARKWAY SOUTH

STA: 18+82 To 20+30

NO.	DATE
1	
2	
3	
4	
5	
6	
7	
8	
9	
10	

NO.	DATE
1	
2	
3	
4	
5	
6	
7	
8	
9	
10	



F.R.W.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	10 895 864	19	65

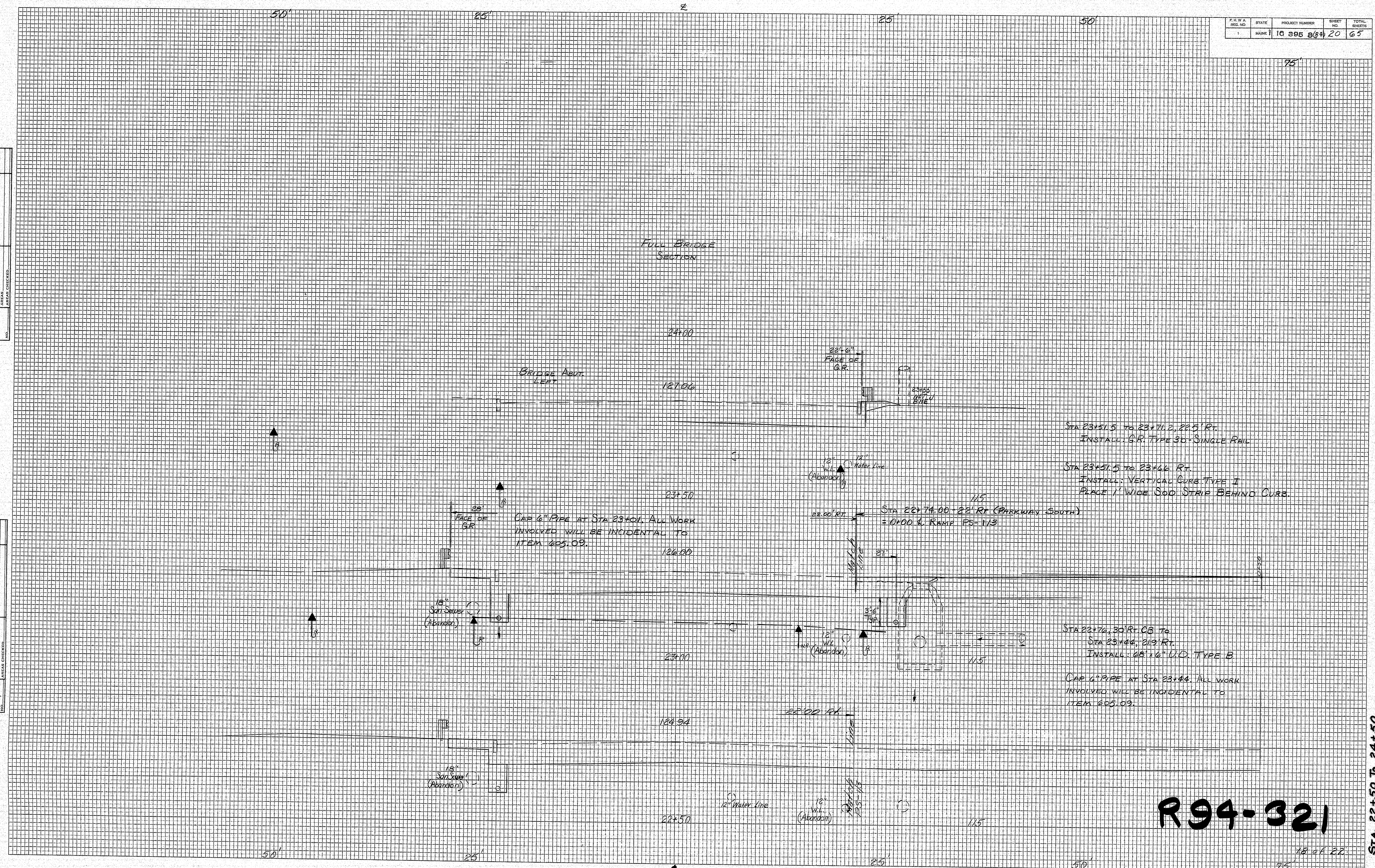
R94-320

PARKWAY SOUTH

STA. 20+50 TO 22+00

FINAL SURVEY	BY	DATE
	SURVEYED	
	PLOTTED	
	TEMP. DATE	
	AREAS	

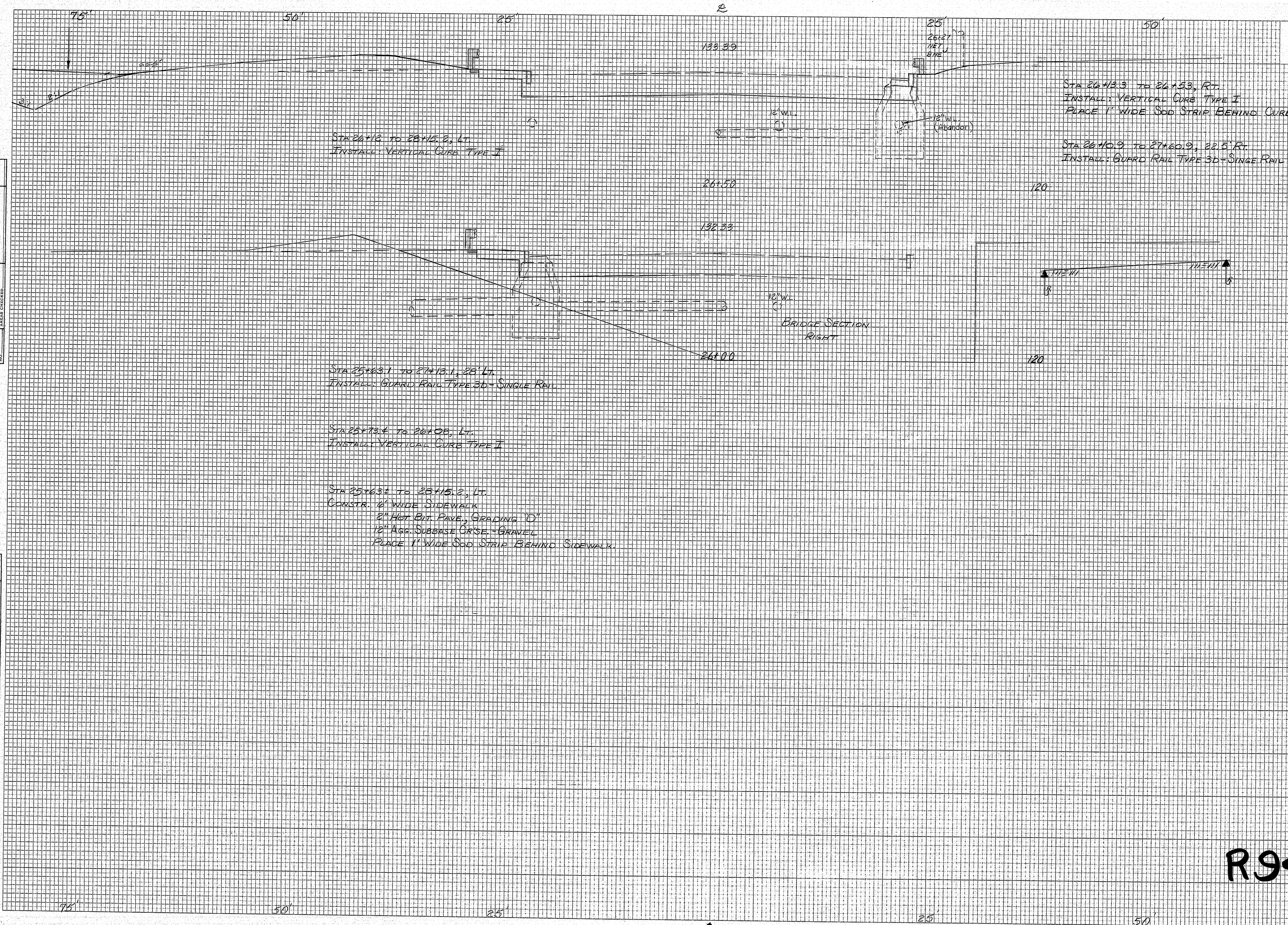
ORIGINAL SURVEY				DATE 8-30-79
SURVEYED			BY <i>Robison & Crew</i>	
PLOTTED			<i>Emery & Kirk</i>	DATE 12-11-81
NOTE BOOK				
TEMPLATE				
AREAS				
395/26				



R94-321

PARKWAY SOUTH

F.H.W.A. RD. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	10 805 8(84) 21	65	



DATE
BY
REVIEWED
DATE
NOTED
DATE
NO.

ORIGINAL SURVEY NO.	DATE	BY	REVIEWED	DATE	BY
39576	8/30/77	Robinson, E. J.	Emery, E. J.	8/30/77	
39576	8/30/77	Emery, E. J.	Emery, E. J.	8/30/77	

R94-322

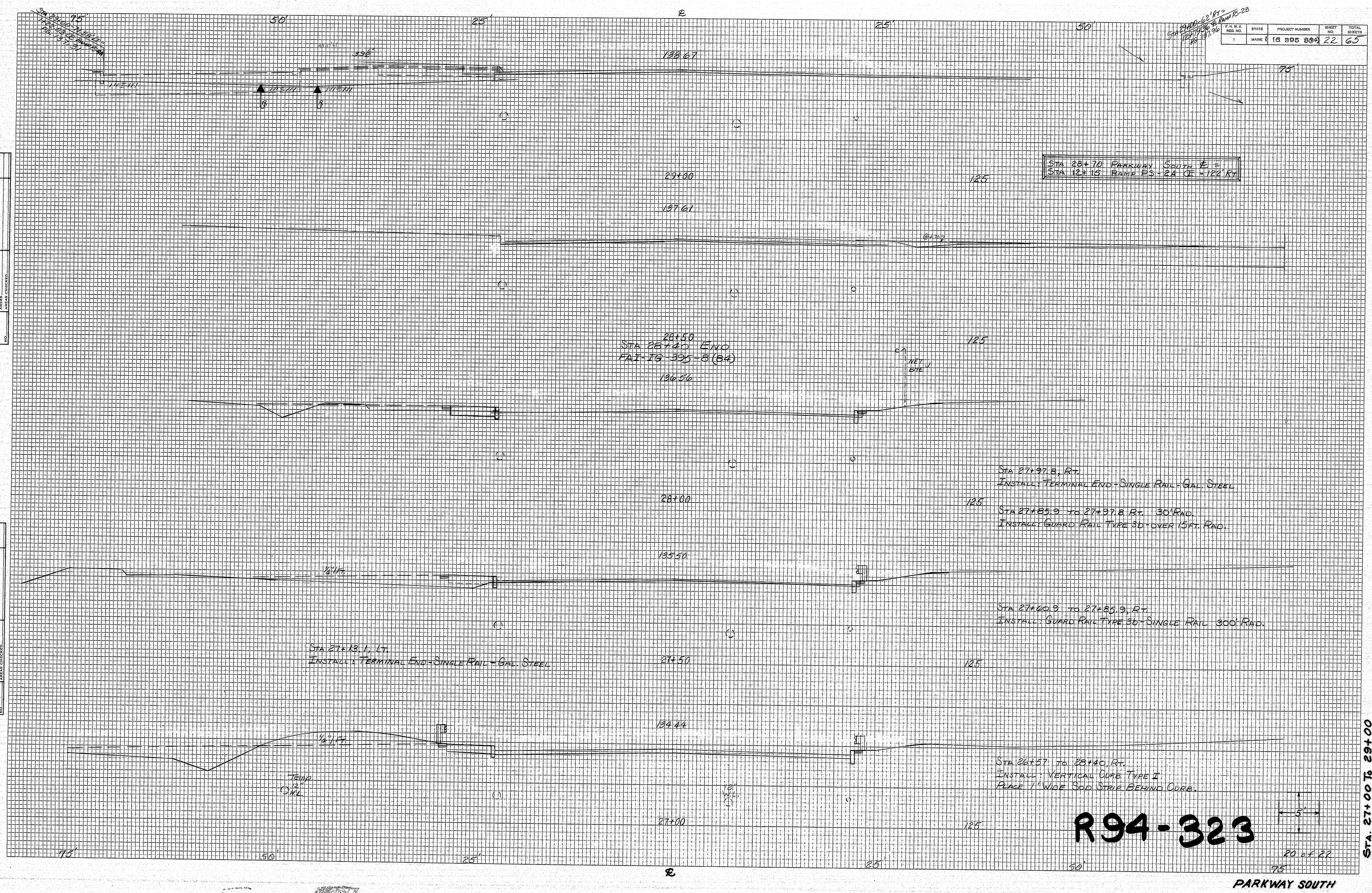
19 of 22
PARKWAY SOUTH

STA. 24+86 TO 26+50

STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
MAINE	16 995 864	22	65

DATE	BY
8-3-79	W. J. L.
8-14-80	W. J. L.

DATE	BY
8-3-79	W. J. L.
8-14-80	W. J. L.

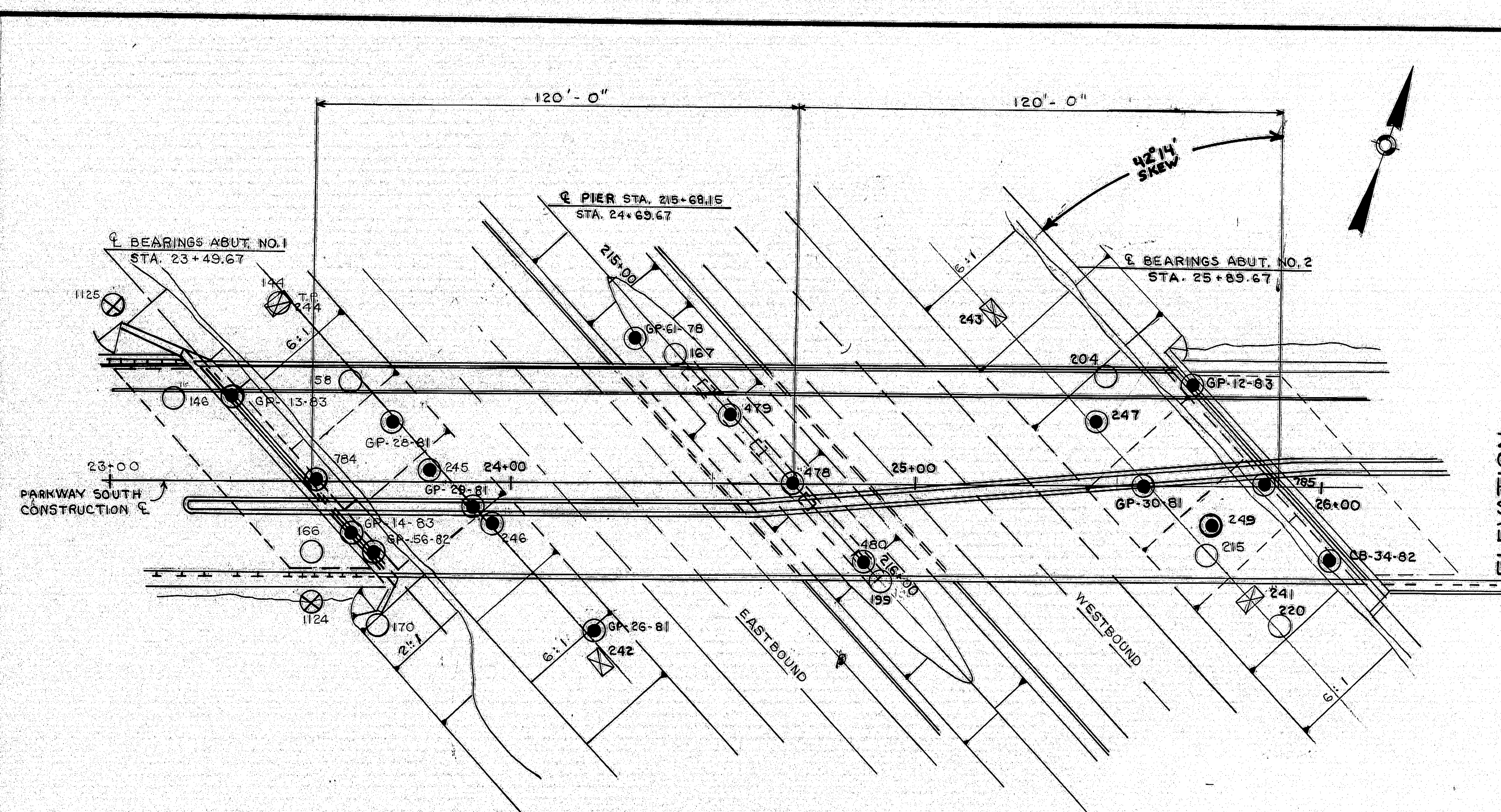


R94-323

STA. 27+00 TO 29+00

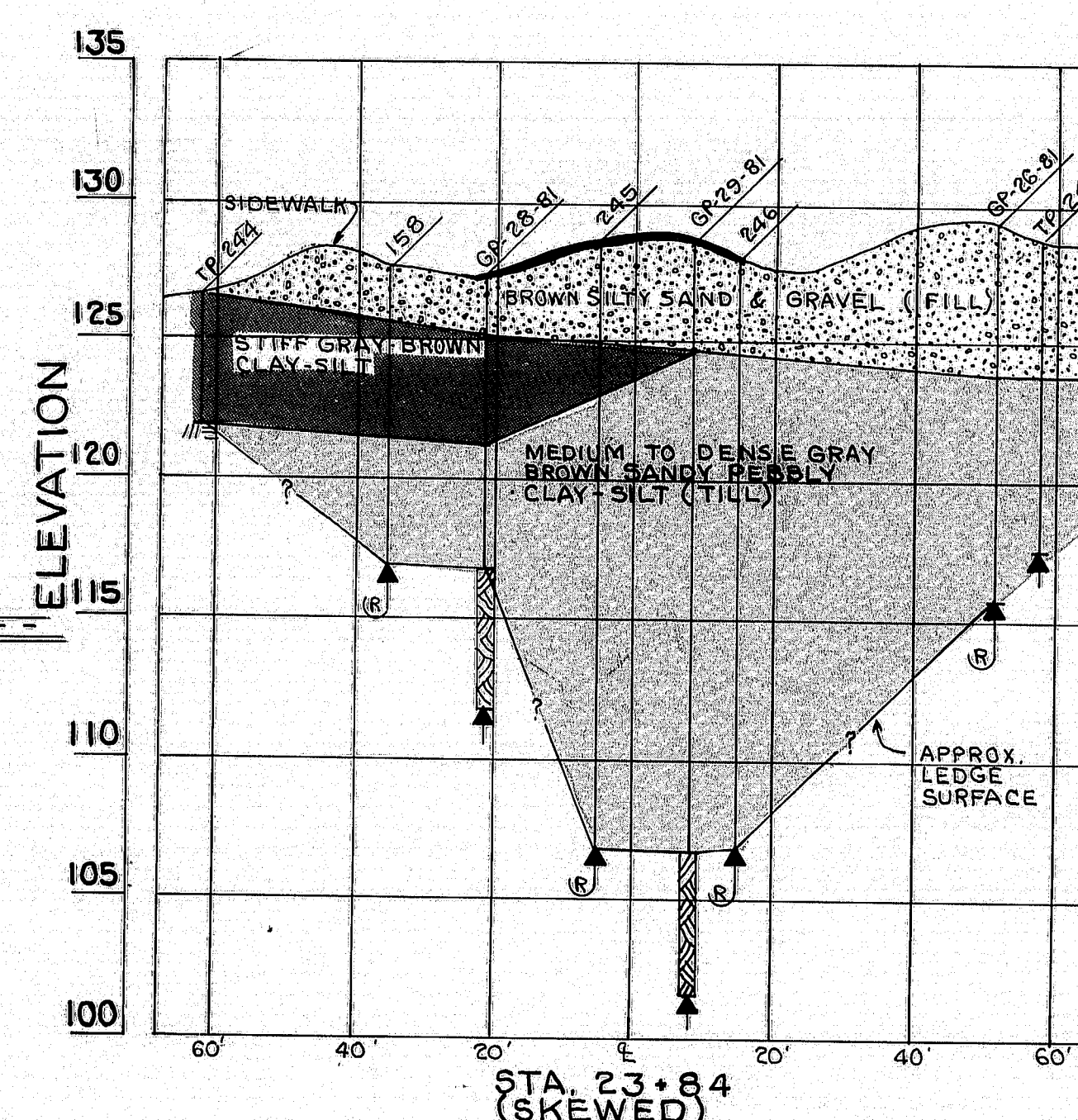
PARKWAY SOUTH

F.H.A. RES. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	395-8(84)	23	65

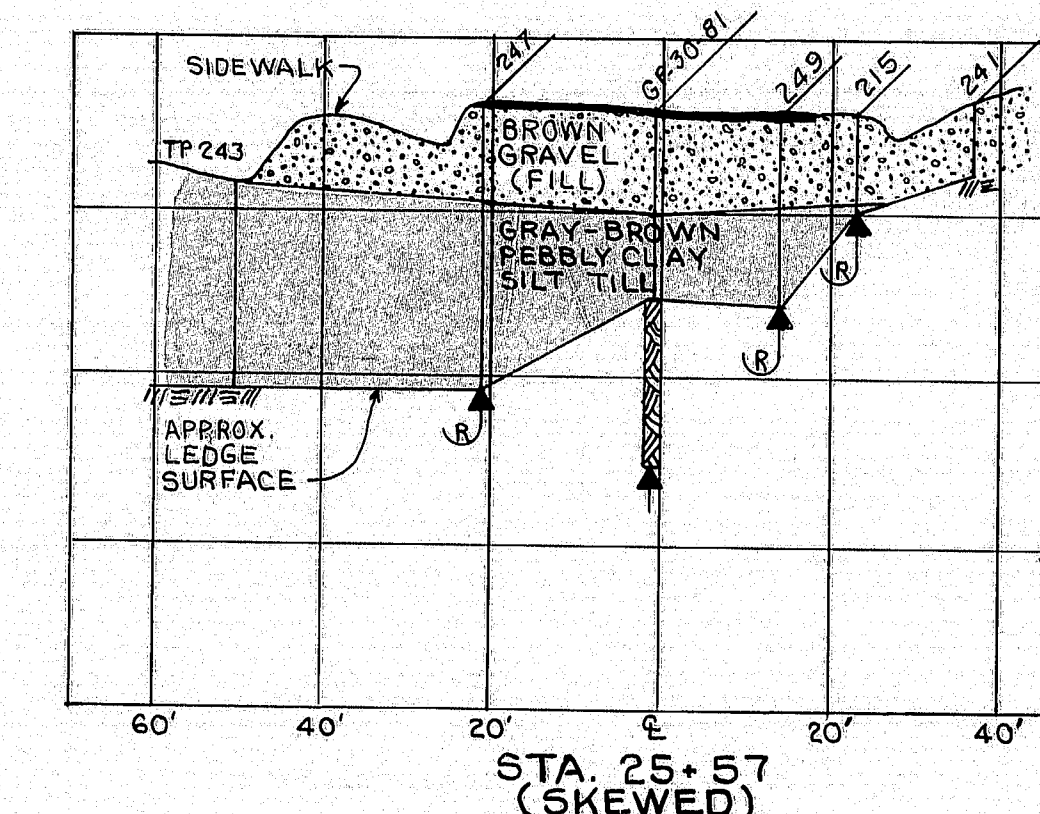


PLAN
SCALE
0 20' 40'

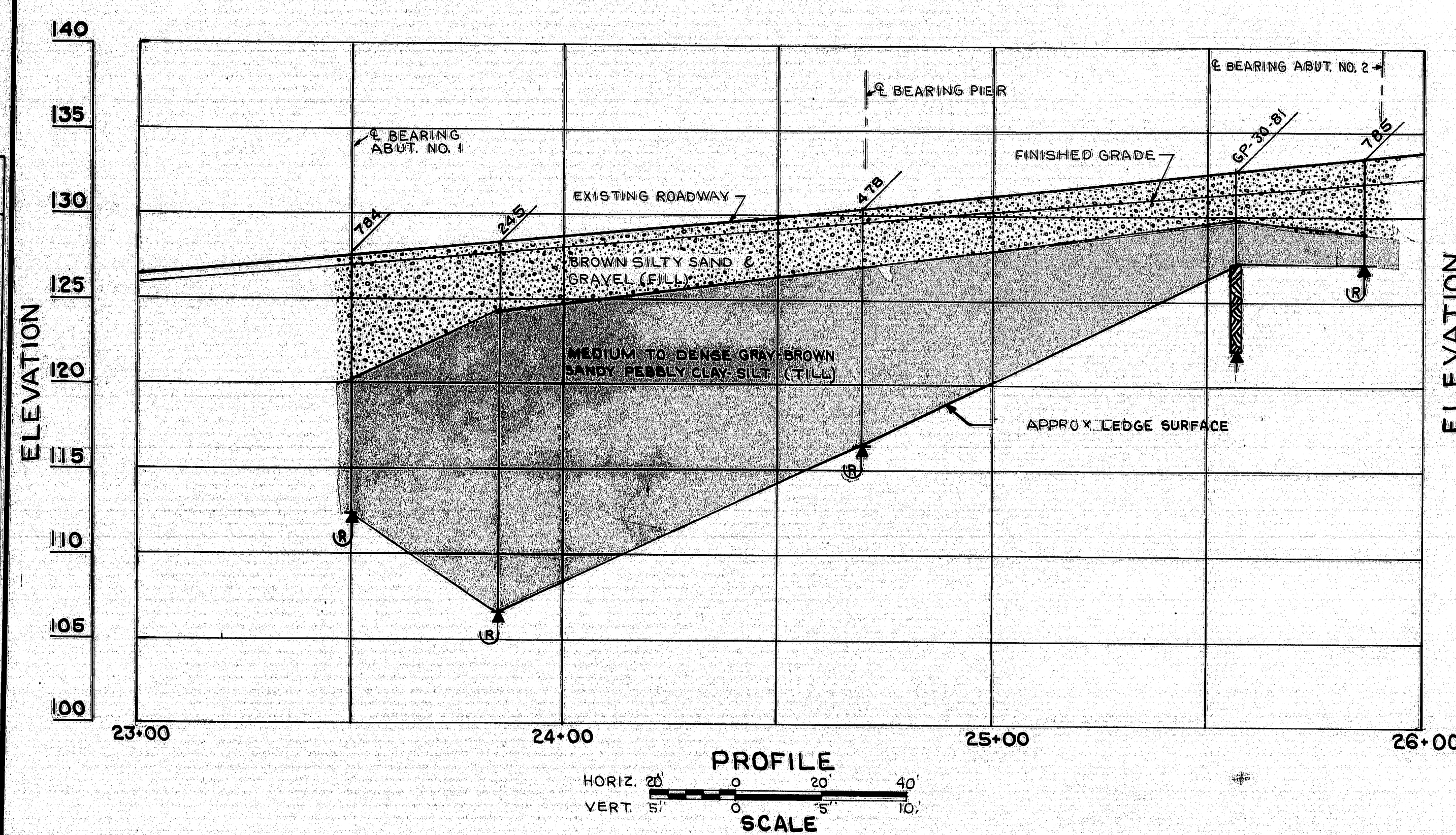
TRANSVERSE SECTIONS



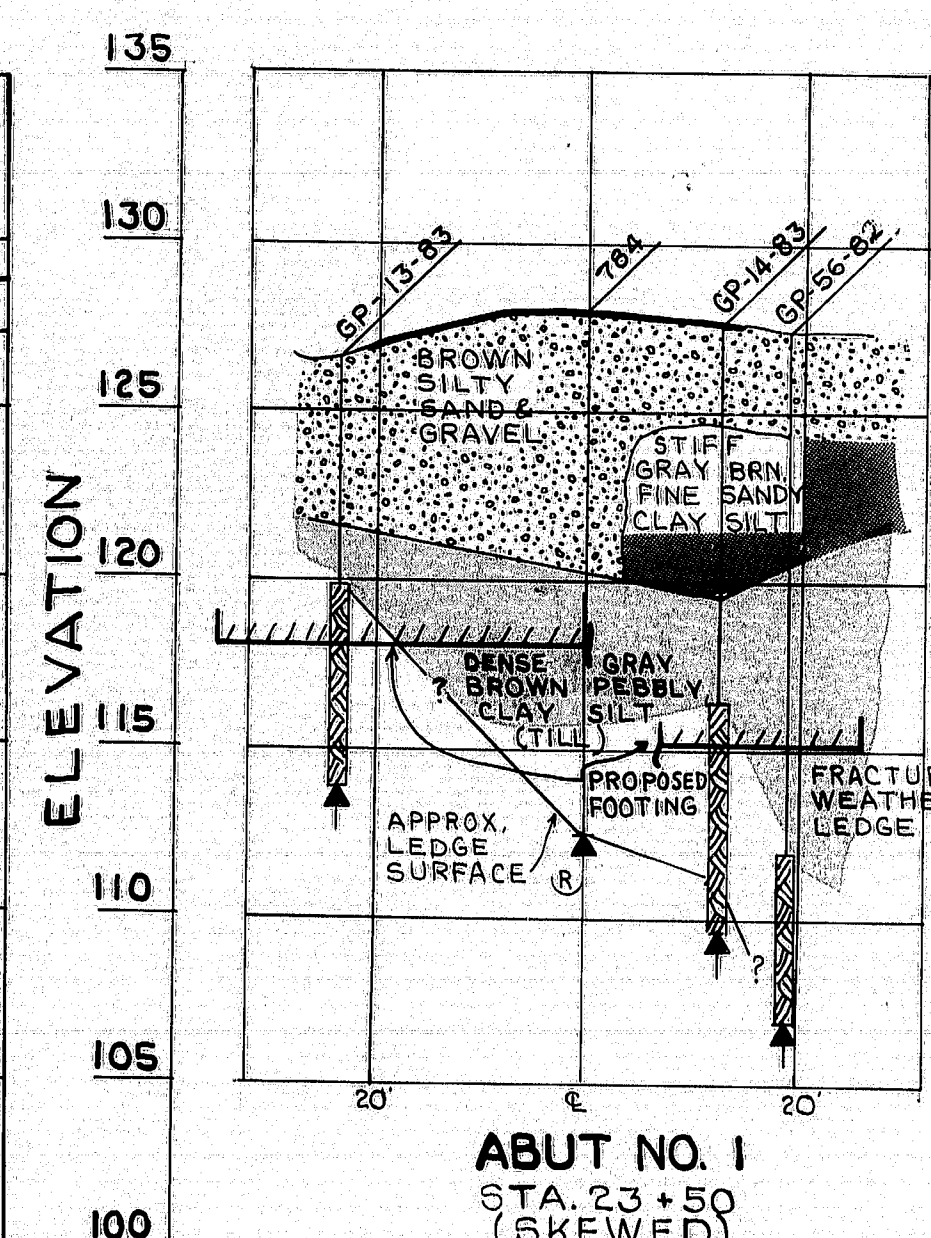
STA. 23+84
(SKEWED)
HORIZ. 20' 0' 20' 40'
VERT. 5' 10'
SCALE



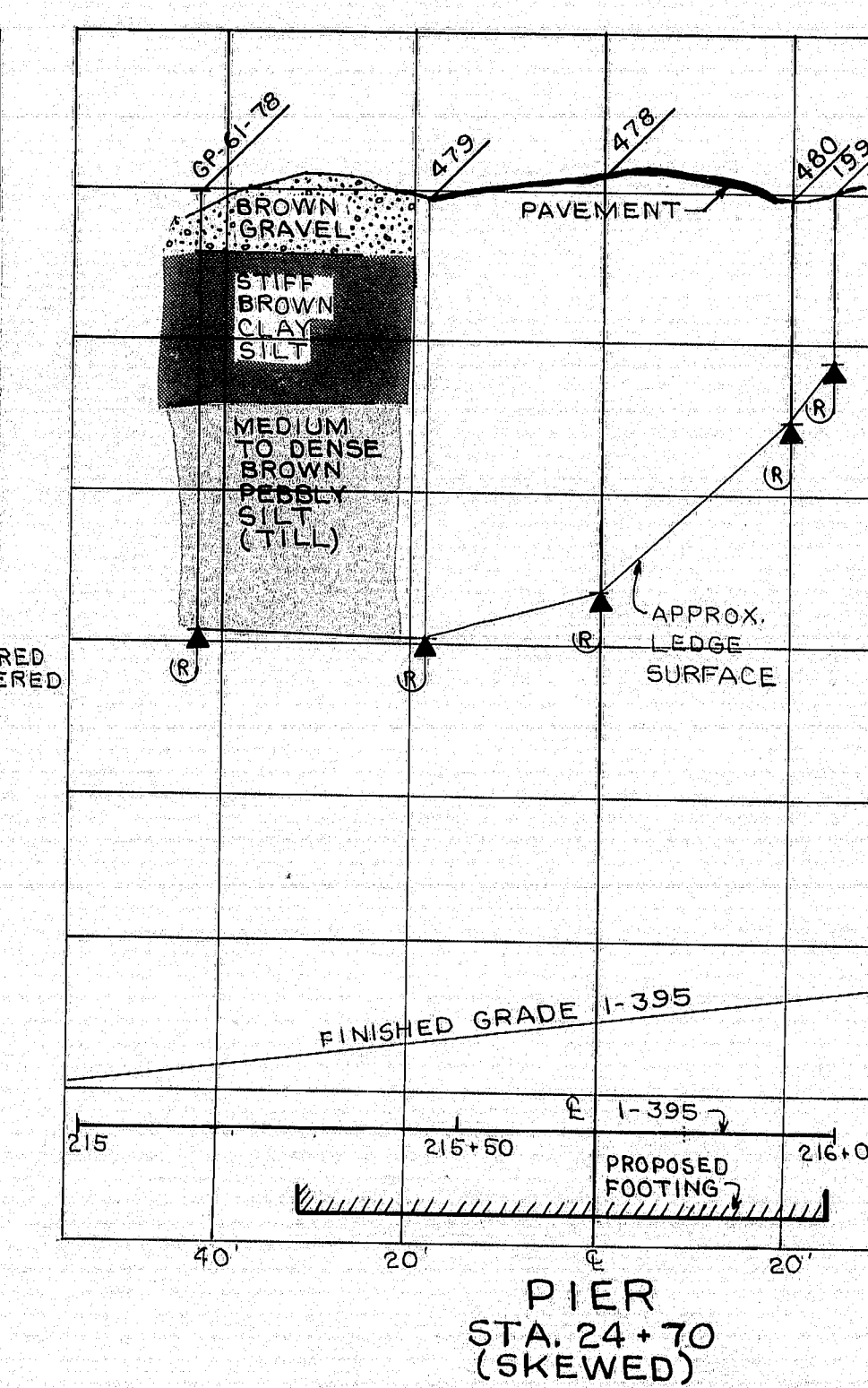
STA. 25+57
(SKEWED)



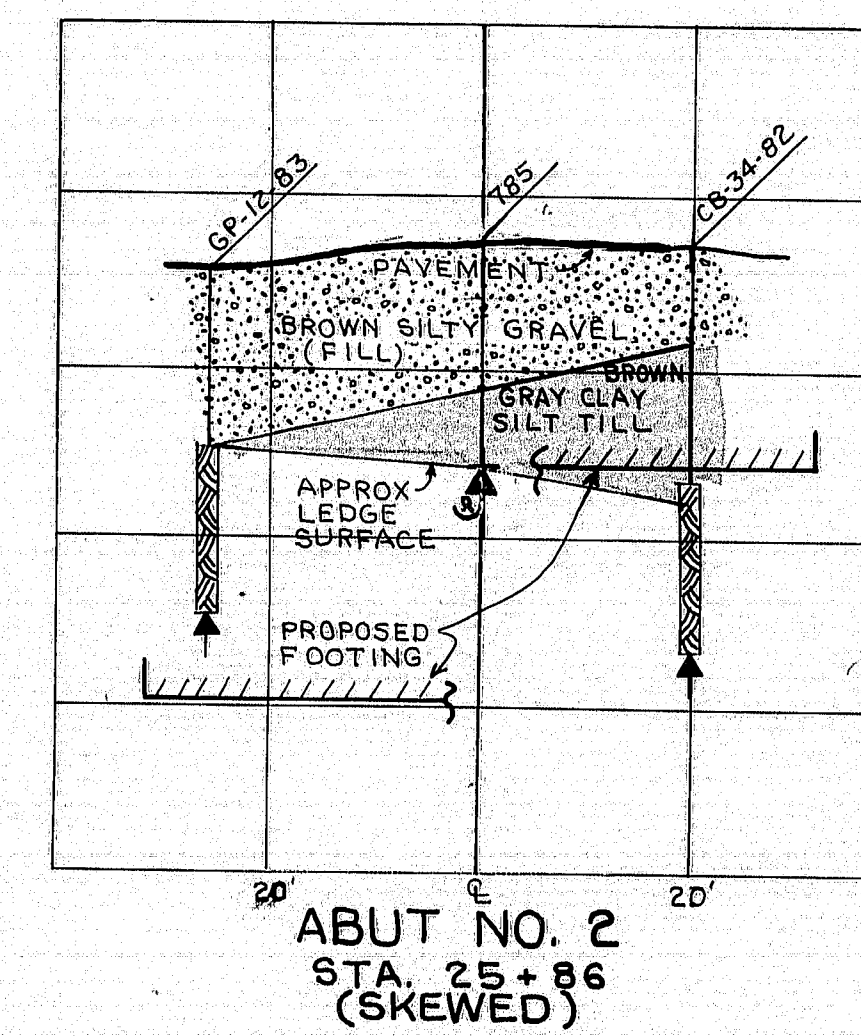
PROFILE
HORIZ. 20' 0' 20' 40'
VERT. 5' 10'
SCALE



ABUT. NO. 1
STA. 23+50
(SKEWED)



PIER
STA. 24+70
(SKEWED)



ABUT. NO. 2
STA. 25+86
(SKEWED)

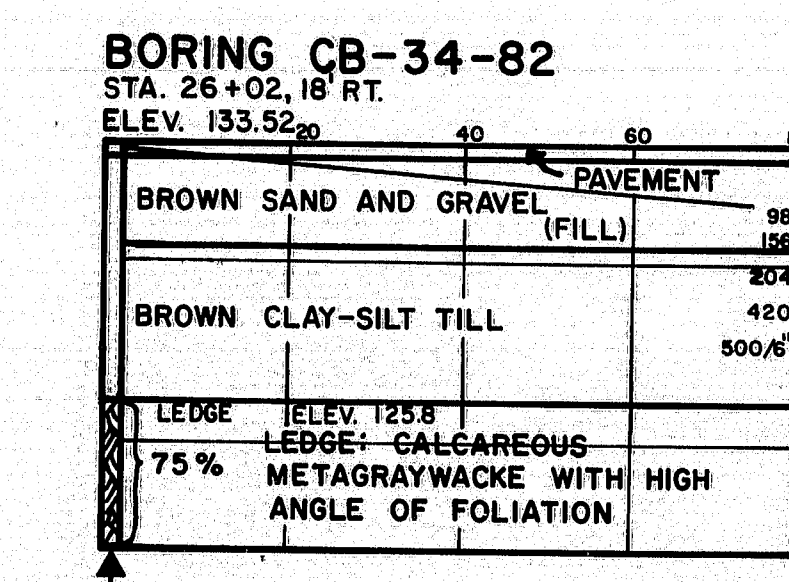
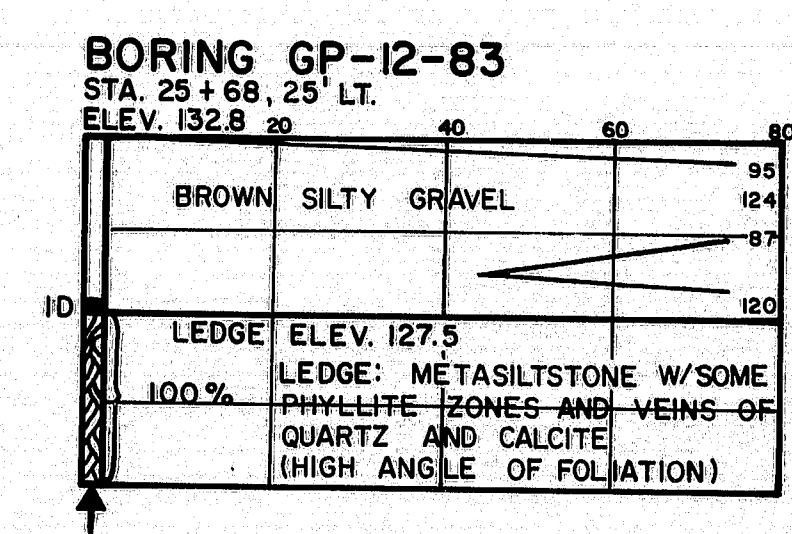
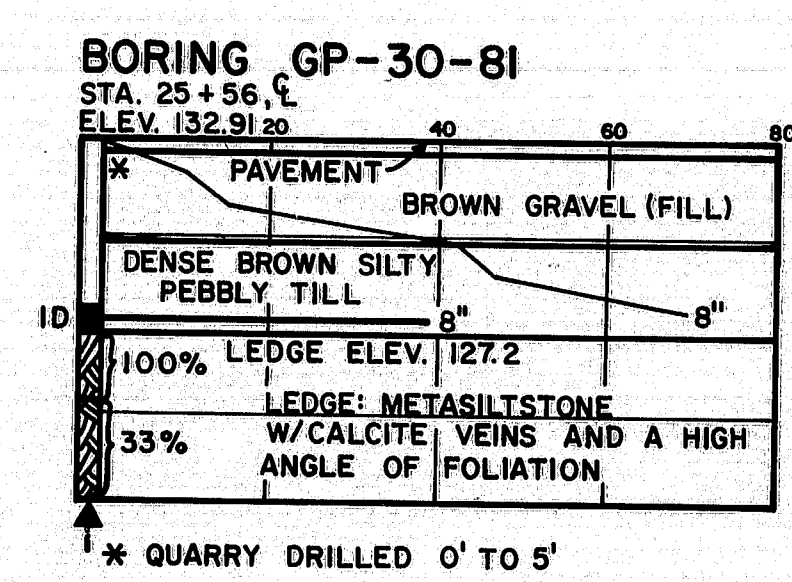
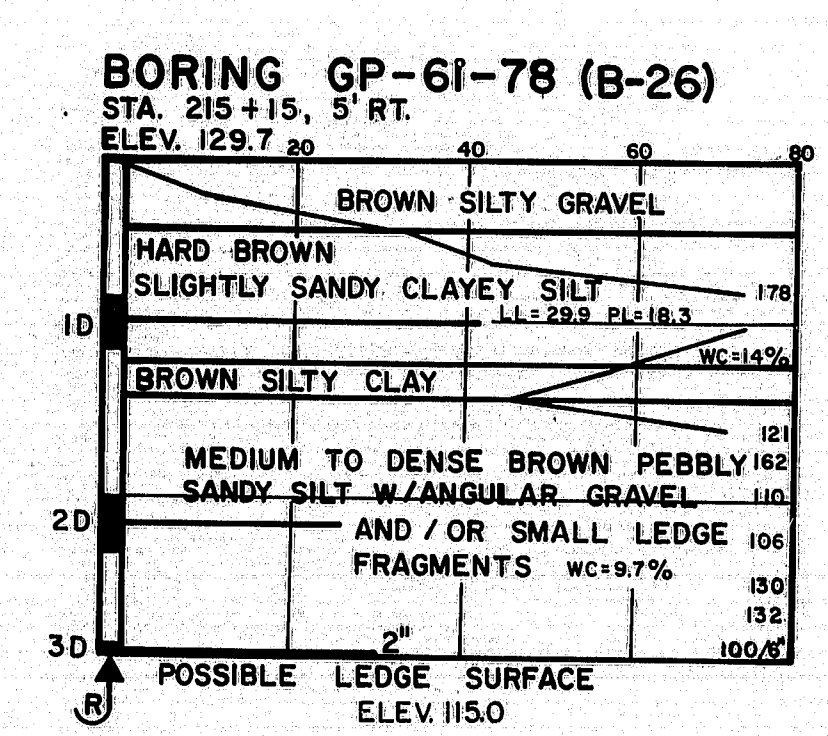
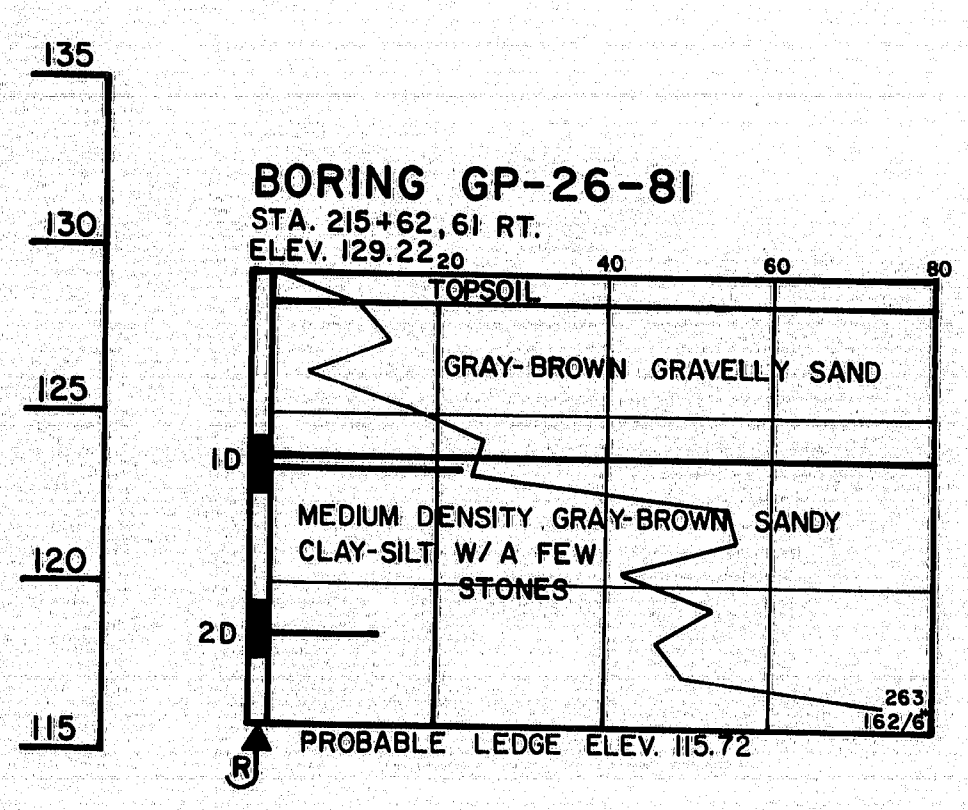
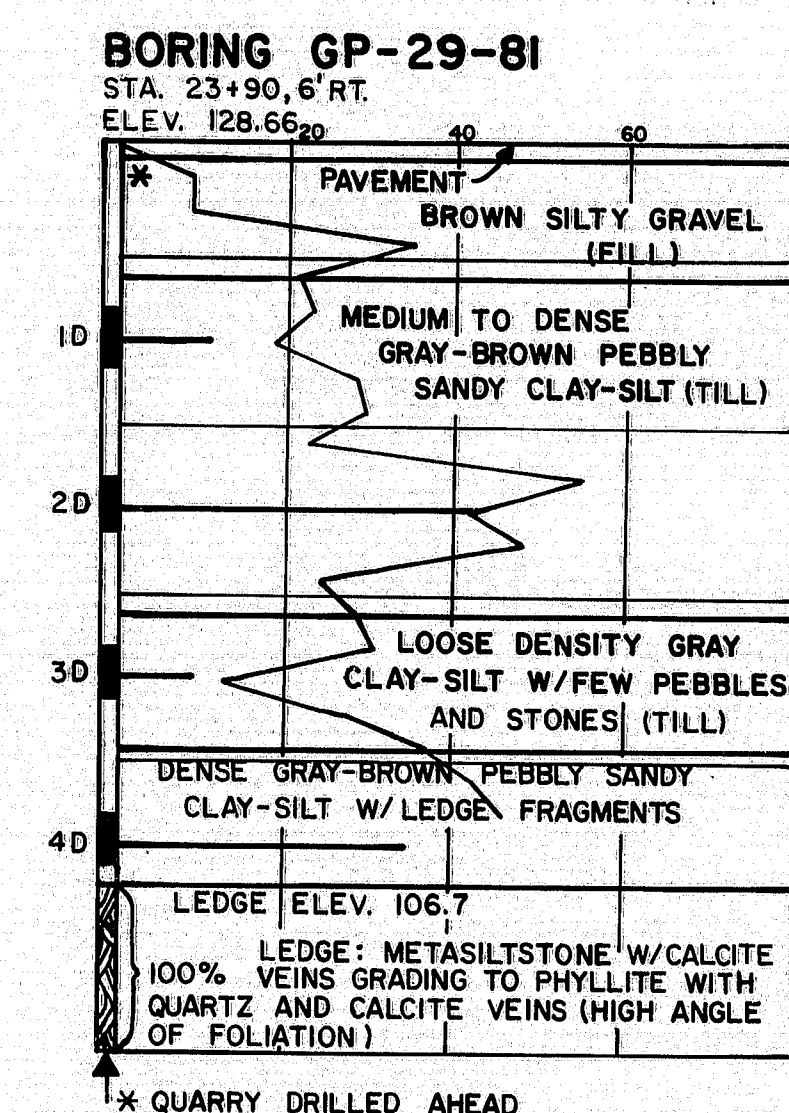
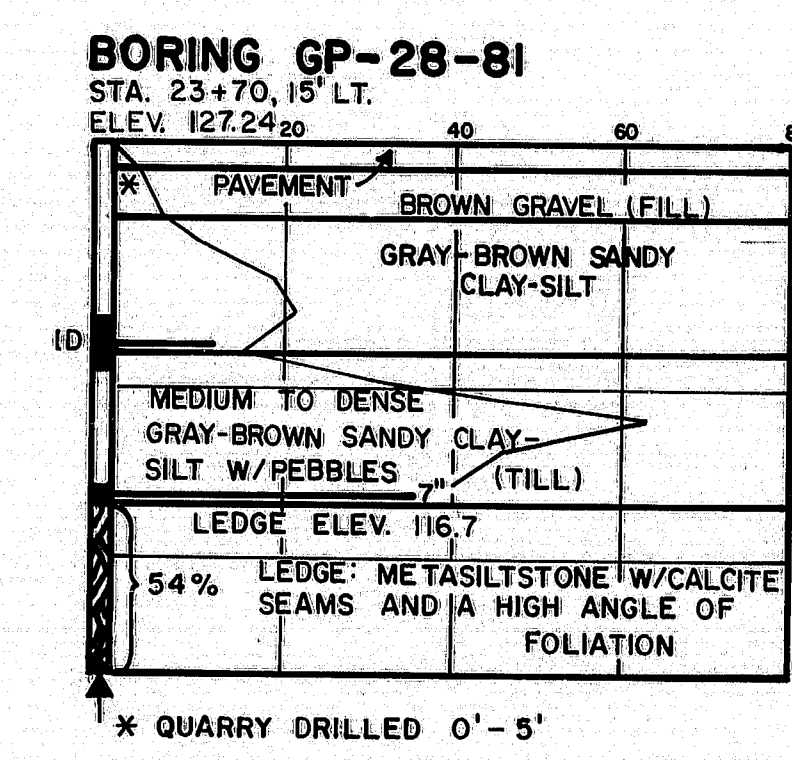
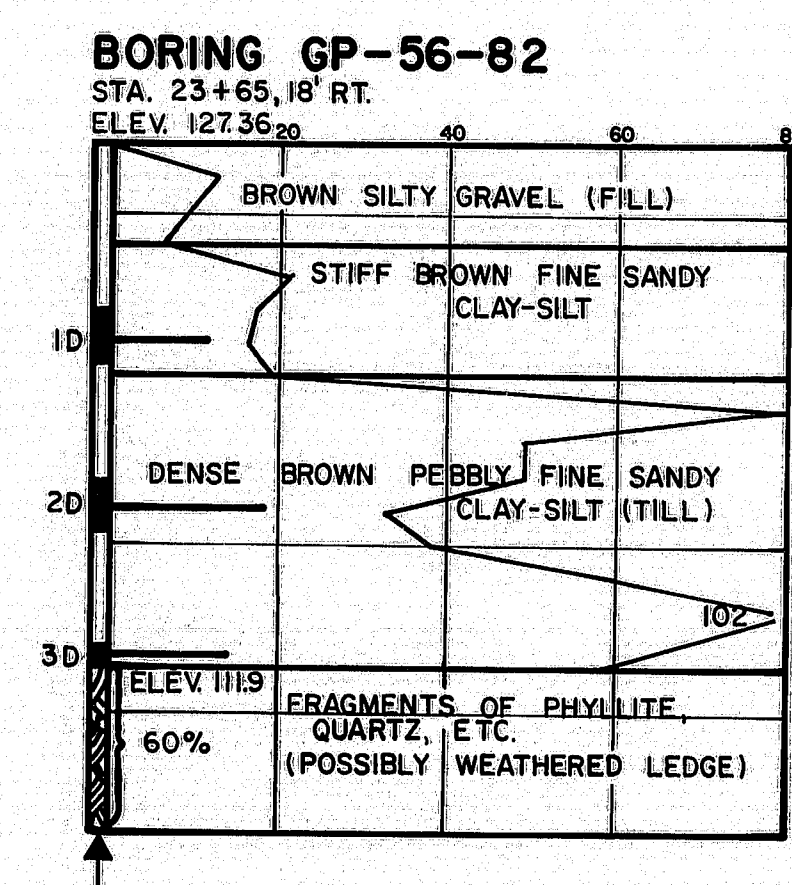
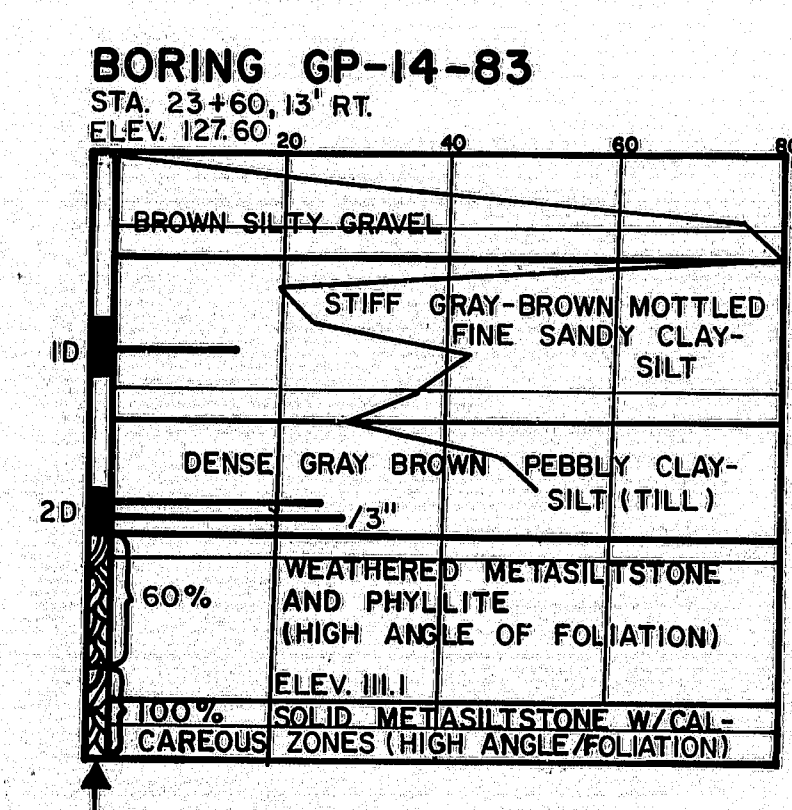
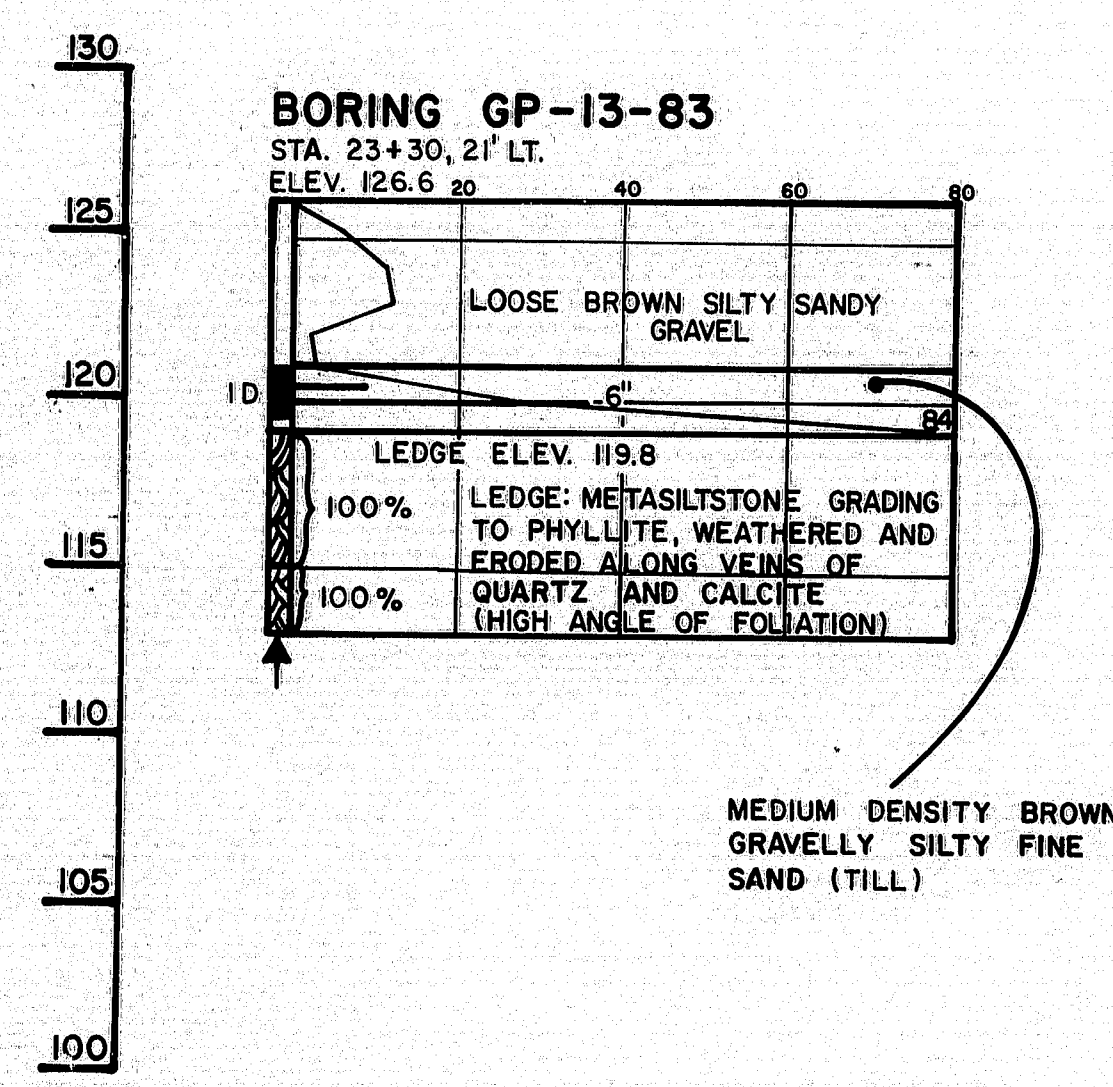
STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

PARKWAY SOUTH BRIDGE
OVER
I-395
IN THE CITY OF
BREWER
PENOBSCOT COUNTY
FOUNDATION SURVEY

R94-324

SHEET 21 OF 22 AUGUSTA, MAINE

F.R.W.A. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	395-8(84)	24	65



- BORING NOTES**
- All samples and vane are made ahead of casing
 - ≡ Number of blows required to drive extra heavy casing one foot with 400 ft. lbs. of energy per blow
 - Location of sample or sample attempt
 - IDS & H Sampler # 1290 1/2
 - Number of blows required to drive spoon or tubing one foot with 350 ft. lbs. of energy per blow
 - ↑ Bottom of boring (may not be bottom of soil strata)
 - ▲ Refusal of drill rods or casing (may not be ledge)
 - ▲ Locations cored by diamond bit and percent recovery of rock

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

PARKWAY SOUTH BRIDGE
OVER
I-395
IN THE CITY OF BREWER
BREWER
PENOBSCOT COUNTY
BORING DETAILS

SHEET 22 OF 22 AUGUSTA, MAINE

R94-325

PROJECT DESIGN ENGINEER	BY	DATE
DESIGN - DETAILED		
CHECKED		
REVISIONS		
FIELD CHANGES		

BORING 44-132-45710